

**MINUTES
CITY OF UKIAH PLANNING COMMISSION
February 14, 2007**

MEMBERS PRESENT

Ken Anderson
Mary Anne Landis
Anne Molgaard
James Mulheren
Judy Pruden

OTHERS PRESENT

Listed below, Respectively

STAFF PRESENT

Dave Lohse, Associate Planner
Cathy Elawadly, Recording Secretary

MEMBERS ABSENT

None

The regular meeting of the City of Ukiah Planning Commission was called to order by Chair Pruden at 6:30 p.m. in the Council Chambers of the Ukiah Civic Center, 300 Seminary Avenue, Ukiah, California. Roll was taken with the results listed above.

2. ROLL CALL

3. PLEDGE OF ALLEGIANCE

Everyone cited the Pledge of Allegiance.

4. SITE VISIT VERIFICATION

Site visit for items 9B and 9C were verified.

5. APPROVAL OF MINUTES - January 10, 2007 & January 24, 2007

Commissioner Anderson referred to page 19, paragraph 3, subsection 5, and requested this comment be stricken.

It was the consensus of the Commissioners present to note that the comments are relevant to the project discussions and recommended they be retained with a public member speaking.

ON A MOTION by Commissioner Mulheren, seconded by Commissioner Landis, it was carried by an all AYE voice vote of the Commissioners present to approval January 10, 2007 minutes, as amended.

ON A MOTION by Commissioner Anderson, seconded by Commissioner Molgaard, it was carried by an all AYE voice vote of the Commissioners present to approval January 24, 2007 minutes, as submitted.

6. COMMENTS FROM AUDIENCE ON NON-AGENDA ITEMS

No one from the audience came forward.

7. APPEAL PROCESS

Chair Pruden read the appeal process. For matters heard at this meeting, the final date for appeal is February 26, 2007.

8. VERIFICATION OF NOTICE

Major Site Development Permit No. 06-19 & Minor Subdivision Map No. 06-18 and Zone Change No. 05-24 and Minor Subdivision Map No. 05-25 were legally noticed in accordance with the provisions of the Ukiah Municipal Code.

Associate Planner Lohse stated there is no public requirement for noticing Major Site Development Permit No. 04-49, since it was an item that was continued.

9. PUBLIC HEARINGS

9A. Major Site Development Permit No. 04-49, as submitted by the Rural Communities Housing Development Corporation (RCHDC) to allow the development of a 32-unit apartment complex on a .84-acre property in the R-3 Zoning District. The complex will be developed for persons with low incomes and includes a 25 percent density increase over prescribed unit density standards. The project is located at 578 Clara Avenue (APN 002-138-05). **TO BE CONTINUED TO MARCH 14, 2007**

Commissioner Molgaard will be unable to attend the regular March 14, 2007 meeting.

PUBLIC HEARING OPENED: 6:38:07 PM

Robin Sunbean expressed concerned that a development near a creek may have a negative impact on its ecological system.

PUBLIC HEARING CLOSED: 6:41:45 PM

ON A MOTION by Commissioner Anderson, seconded by Commissioner Landis, it was carried by an all AYE voice vote of the Commissioners present to continue Major Site Development Permit No. 04-49 to the regular March 14, 2007 meeting.

9B. Major Site Development Permit No. 06-19 & Minor Subdivision Map No. 06-18, as submitted by Ruff & Associates Rite-Aid Corporation, to allow the construction of a 17,026 square foot drugstore to replace the existing Rite-Aid drugstore, which would be demolished once the new store is open for business, and divide the 2.5-acre site into two lots with gross areas of 71,026 square feet and 38,001 square feet. The project is located at 680 South State Street (APN 002-301-43), on the northwest corner of its intersection with West Gobbi Street.

Associate Planner Lohse briefly highlighted the staff report as follows:

- Approval of proposed Minor Subdivision Map will allow the division of the existing subject property into Parcels One and Two, respectively.
- Approval of Site Development Permit would allow the construction of a new Rite Aid store on proposed Parcel One on the eastern portion of the site. The remaining Parcel 2 would be vacant when the existing Rite Aid store is demolished.
- The new Rite Aid store will have a drive thru pharmacy on the north side of the building and an entrance on the southeast corner.
- The site conditions are explained on pages 3 and 4 of the staff report that provide a description of the building orientation, facades, roof and window treatment and other design amenities and characteristics, as well as a description of the building in relation to the landscaping, parking facilities, planter areas, driveways for ingress and egress to the site and access for use of the drive-thru pharmacy, and pedestrian orientations.

- The site plans and redesign objective for the new store were to formulate a design in keeping with the fact that the subject properties are located in an urbanized area of Ukiah that has been developed with retail and professional offices to the south, the Safeway supermarket to the east, a motel to the north and low to high-density residential to the west, intended to aesthetically complement the other buildings in the neighborhood and comply with the Downtown Commercial Design Guidelines.
- An Initial Study was prepared in accordance with CEQA provisions in which it was determined that the proposed project would cause potentially significant environmental effects. Specific measures designed to mitigate the effects cited to levels that would be less than significant have been incorporated into the Mitigation Monitoring Program and included in the Conditions of Approval for the project.
- Staff addressed the issue of increased traffic impacts and considered potential impacts from the drug store and drive-thru pharmacy, as well as future high density residential or retail/commercial uses for the Parcel Two, and concluded no impacts would result. In fact, staff determined traffic impacts would actually be reduced with the new store.
- Staff's analysis determined the proposed new development is consistent with the Ukiah General Plan, the C-1 Zoning District with regard to building height, setbacks, parking, maximum lot coverage, and landscaping/lighting as provided for on pages 5 through 7 of the staff report.

The retail store proposed in this project is required to provide one parking stall for every 300 square feet of gross floor area, which equates to a minimum of 58 parking stalls. Project plans indicate that the site will have at least 61 parking stalls in rows of parking along the east, south, and west sides of the building.

Staff also notes the project plans also show a bike rack will be installed in the planter area proposed near the building's entrance.

The C-1 landscape criteria will require the submittal of a lighting plan.

The proposed landscaping does not appear to meet the 20 percent required coverage ratio for the C-1 zoning district. However, staff noted the majority of the landscaping will be live, which exceeds the 10 percent requirement for this type of landscaping.

- The new store is also consistent with the Commercial Design Guidelines relative to site features, parking, pedestrian orientation, compatibility with surrounding land uses, coordination with adjacent properties to share facilities such as driveways, parking areas, pedestrian plazas, and walkways, setback, building materials, color schemes, roof facades, windows, facades and store front openings, rear entries, sign design, pedestrian circulation, lighting and landscaped design as outlined on pages 7 thru 10 of the staff report.
- The project is consistent with the Airport Master Plan and Subdivision Ordinance standards for Type VI subdivisions.
- Building and site design issues have been addressed with the applicants and included in the recommended project conditions.
- Staff supports approval of the building and site design.

Mike Johnson and Jason Brennen provided a PowerPoint presentation on the site layout for the project and noted the architects designed a store that the community would welcome and effectively be a good fit for Ukiah's urban fabric and/or small town urban par tee. The proposed store is smaller and has less parking than the existing store. Design features from

Ukiah's State Street downtown buildings such as brick facades, stone cornices, and other relative features were utilized. The building design made use of offset walls, varied roof lines, treatments and differentiated brick masonry wall coverings and colorings to make the facade appear more like a street front composed of a series of buildings. The intent was to maintain that "old store front theme" incorporating deep awnings and divided windows. Rite Aid desires to create a store having a people-friendly site design. A front corner plaza has been designed for the street corner at Gobbi and State Streets, including a plaza at the entrance and a meandering sidewalk on South State Street. Parking is divided between the front of the building on State Street and also behind the building giving the appearance of less parking at the site. There will be trellises on Gobbi and State Streets that will shade all of the pedestrian walkways, as well as add a "green" element to the facade. The pedestrian walkways were specifically designed for safe access to the store. The driveways are narrowed where the pedestrians will walk across making access more approachable. Rite Aid has deeded approximately 200 square feet to the City for repairs to the intersection at Gobbi and State Streets for ease of vehicular navigation. Also, with the proposed subdivision a second lot has been created for future development.

Commissioner Anderson supports the use of permeable paving materials to help lessen water runoff in parking lots and inquired if the Rite Aid plans include this use.

Jason Brennen stated the type of paving materials for the parking lot has not been determined. It is likely the plans would provide for standard asphalt because it wears better versus permeable materials since parking lots are high traffic zones. There will be less asphalt than the existing store because the parking lot will be smaller, which would allow for more green space.

Commissioner Landis noted as underutilized parcels are now being considered for development, she supports the application of quality design and use of materials that highly complement neighborhoods, benefit the community, and protect the environment. She also supports the use of permeable paving materials for commercial parking lots and recommends this concept be further explored.

Commissioner Landis addressed the building design and favors the angled entryway. She questioned the reason why the building was not set closer to the street and why there is so much parking before a person reaches the street. The design of projects is at a new juncture where the application of Form Base Zoning would require quality and aesthetically pleasing developments to make the City as attractive and pedestrian friendly as possible. While the new urban design ideas, such as the application of Smart Growth concepts, the Green program, and Form Base Zoning are not yet in place for this community, consideration can be given to projects to begin the process to promote quality projects. Bringing the building closer to the sidewalk would help people want to approach and would make the building "more friendly" to a pedestrian. She is pleased the building would be closer to the street than the existing building. She inquired why the some of the parking could not be oriented to the rear of the building, since the southern end is part of the City's gateway. She supports that the community, planners, developers, architects and other agencies/organizations plan properly for the future so that developments, building improvements/renovations are well done for the good of Ukiah. While she appreciates the facades and the variety of materials used to break up the mass, she generally views the design as a box-like commercial structure. She recommends consideration be given to reorienting the building closer to the street, moving the parking more toward the rear of the building, adding windows and treatments, increase the planter area so that vegetation and trees would thrive, and other

amenities that would provide an aesthetically pleasing urban building that complements and enhances some of the historical flavor of the downtown and make the building more "friendly" to the community. She noted the redesign is certainly "more friendly" than the existing site design. She appreciates the initiative taken to improve the existing site and generally supports the project. The community would likely be pleased long term, if the aesthetics and building orientation were reevaluated to better suit the urbanized concept and the accompanying green program designs and materials. The design is essentially one of a typical drug store.

Commissioner Molgaard inquired whether thought has been given to energy efficiency design and noted the plain mass walls do not have windows or some type of treatment to break up the mass. Additional windows would allow for more light and less consumption of energy and associated costs, as well as provide for a more aesthetic pleasing appearance.

Jason Brennen referred to the site plans and reiterated a good portion of the parking is in the rear of the proposed building. The vehicle circulation is through the rear of the building and away from the building front although access is from the front entrance off the street. The building would have two front entrances to assist with effective circulation on-site. Pedestrian safety and accessibility were a major concern where considerable design efforts were made in this regard. As far as increasing the number of windows, the Rite Aid corporation has limitations as to the number of windows it can incorporate relative to window display and merchandise. The applicants desire to provide as much "green" on the site as possible, as well as provide for adequate shade with the trellises and awnings. The windows were designed as large as possible to create that "friendly" atmosphere.

Commissioner Molgaard inquired what would happen to the merchandise if additional windows were placed at the top of some walls, excluding that section of the wall where the pharmacy would be located.

Jason Brennen stated Rite Aid has a particular way merchandise is displayed and the window height is a corporate decision. He would consult with Rite Aid whether the addition of windows is a possibility. He stated the applicants and architects worked diligently to get the project to this point.

A general discussion followed regarding the 200-foot easement at the corner of South State Street and Gobbi Street that was deeded by Rite Aid to the City for improvements to this intersection to help make the pathway at this corner smoother and safer for vehicles.

There was further discussion regarding the bicycle path along Gobbi Street and the proposed bicycle rack at the entrance of the building. Staff recommended the rack be removed from the planter area and supports that the bicycle rack be put under a canopy. Staff further recommends an additional bike storage facility be installed for employees within the building or in another covered area.

Commissioner Molgaard inquired whether the 200-foot easement at the intersection of Gobbi and State Streets result in a right-turn lane heading south on State Street.

Associate Planner Lohse stated the easement will eventually assist with realignment at the intersection and allow for better turn lanes and the bike lane will also be improved.

Commissioner Molgaard further inquired whether there would be a separate right-turn only lane when traveling south.

Associate Planner Lohse consulted with the City Public Works Department about this matter, and it was noted a separate right-turn only lane traveling east and turning onto State Street may eventually be created with the extra width gained from the easement.

Commissioner Molgaard desires that the right-turn only lane be a part of this approval process.

Associate Planner Lohse stated it would be difficult to tie this element to the proposed project because the increase in traffic would not be substantial enough to state that any additional impacts created would be more than what already exists. There is simply not a strong nexus to tie this project to the street improvement relative to approval or denial. The project, will however, allow for an improvement that will be much more effective than the current conditions.

Chair Pruden commented the Citywide Traffic Study being conducted will address all key intersections where certain improvements will be recommended. The intersection at Gobbi and State Streets will likely be changed as a result of the traffic study regardless of whether the Rite Aid project is approved or not.

Commissioner Anderson inquired whether large corporations in particular consider the use of energy efficient alternatives for new developments.

Jason Brenner has no knowledge of how large corporations plan in this regard.

Commissioner Landis is hopeful the landscaping and trees fronting State Street will be appropriately maintained unlike the lack of appropriate care of the previous vegetation on the site. She inquired whether a maintenance plan will be in effect for the new Rite Aid store so that the site and corner of the property will be consistently beautified.

Jason Brenner specifically addressed the trees that front State Street and stated past City policy required that Rite Aid maintain these trees.

Associated Planner Lohse stated a project condition requires continued maintenance and enforced by the City.

PUBLIC HEARING OPENED: 7:13:38 PM

Mike Johnson supports approval of the project because it will be a significant upgrade to the neighborhood.

Judy Boyle owns property adjacent to the existing store and is hopeful the nuisance, trespass, and vandalism issues that have occurred in the past in around the common fence between her property and the existing store will be specifically addressed and resolved. She commented the proposed new store will be a significant improvement for the area.

PUBLIC HEARING CLOSED: 7:18:35 PM

The Commission discussed the project as follows:

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- The use of Parcel Two is unknown at this time.

Commissioner Landis supports shared use of parking facilities and driveways for the two parcels and less parking when it is unnecessary.

Chair Pruden acknowledged this comment and noted with approval of the subdivision control of Parcel Two is essentially lost and ultimately sold as a separate parcel for future development.

The Commission supports approval of the subdivision.

- The Commission supports approval of the proposed Mitigated Negative Declaration for the project.
- In terms of the Major Site Development Permit, the Commission recommended possible conversion of the three extra parking stalls proposed to covered bike areas.

Chair Pruden commented on staff's recommendation that the proposed bike rack is removed from the planter area and placed on a paved and covered area that will encourage its use throughout the year and that the applicants design a secure bike parking area for employees. She asked staff if the intent is to install a bike rack at the building entrance.

Associate Planner Lohse desires the rack be installed as close to the building and pedestrian access as possible.

While the applicants have not submitted a lighting plan, the Commission desires that the lighting for the parking lot be subdued and downcast and decorative.

Chair Pruden referred to page 10 of the staff report, and noted staff recommends the pole sign is between eight and ten feet in height. More specifically, she recommends the sign has a maximum height of 8 feet with allowance for a two-foot decorative element on top. Large signs are not necessary in the Downtown because the community typically knows where commercial establishments are located.

Commissioner Mulheren concurs with staff's recommendation provided placement of the sign does not obstruct visibility.

The Commission recommends the sign be placed so as to allow for a clear line-of-sight.

Staff noted the sign program shown on the plans indicate the retention of an existing pole sign and the proposed use of additional monument signs along both the State Street and Gobbi Street frontages for a total of three freestanding signs on the site. The UMC clearly limits commercial developments to a single freestanding sign and the construction of two new monument signs would require the approval of a variance that staff is not prepared to support. The applicants have indicated they will remove the existing pole sign and replace it with a single freestanding sign on a monument base that will be located in the planter area proposed for the southeast corner of the site, as provided for Condition of Approval No. 10. The proposed

monument sign shows that it would be mounted on a small brick-covered base and substantially lower than the existing pole sign on the site. Comparably, the height is consistent with other monument signs in the neighborhood.

It was the consensus of the Commissioners present to support approval of the proposed monument sign.

Chair Pruden addressed the landscaping on the site, and supports retention of the existing "urban forest" landscaping on South State Street. While these trees are generally healthy and viable, they need to be better maintained to allow for a larger canopy. "The Community Forest Management Plan calls for the protection of existing trees on properties proposed for development. The protection of significant existing trees and specimens both native and non-native and inclusion of new trees within a comprehensive site development program is generally beneficial to the City and the property owners directly." The two-foot planter strips in the parking lot would not be able to prosper into a viable urban forest. She would be willing to relinquish some of the new trees to be able to retain the trees along State Street. The trees proposed for the front of the building will not mature to the degree of existing trees that front State Street. She understands that some of the trees along Gobbi Street may not be retained.

There was discussion about parking lot trees being healthier in areas that are paved with permeably materials.

Chair Pruden stated trees planted in small planter areas typically do not flourish and mature as they should. A minimum of 25 square feet is necessary in order for a medium-sized tree to adequately grow.

Associate Planner Lohse referred to Site Development Permit Condition of Approval No. 49 (g) that requires, "the submittal of a report on the health and feasibility of retaining the existing trees located in the planter area abutting State Street."

The Commission supports planting trees for the purpose of creating an urban forest in areas and with conditions that will allow them to thrive.

Commissioner Landis referred to page 5 of the staff report where staff finds that the parking ratio is adequate and supports the removal of three extra stalls to allow the expansion of the perimeter landscaping strip proposed along the southern side of the lot. She is hopeful an area having the necessary depth to grow larger and healthier trees is accessible.

Chair Pruden recommended adding the square footage from the three excess parking stalls to the planter areas allowing for six to eight viable planters in which the trees can grow to a sizable height.

It was the consensus of the Commission to support retention of the deciduous trees along State Street and add the square footage from the three excess parking stalls to the planter areas allowing for between six and eight viable planters.

- **Chair Pruden** addressed the general design features, and stated it is a commercial building typical of drug stores seen nationwide.
- **Commissioner Landis** did some research on 'building and context' for new construction compatible to the community, and noted that architecture is the most reasonable expression of local history relative to its contribution to the community. A building can be constructed to be distinctive and nicely designed allowing for an interesting mix for the streetscape. The Rite Aid site may at some point be viewed as a more historic area in the Downtown making it more important for the new building to complement the historical aspects of older buildings in the neighborhood and feature the necessary design elements to maintain and preserve these design characteristics. She supports the building design incorporate particular architectural elements, such as the detail of the elevations to avoid the standard drugstore appearance. While the new store, as currently designed, is a nice improvement, the proposed building could be a drugstore anywhere in the United States. She favors the front entrance orientation and design elements for the corner of the site and she understands that considerable design changes have been made to the initial design. In her opinion, the proposed store resembles the other drug stores in Ukiah, and does not effectively add to the architectural legacy of the community.
- **Chair Pruden** questioned how much more fenestration and/or project revisions can be requested to be in substantial compliance without the applicants having to go back through the discretionary review process.

Associate Planner Lohse stated in order to avoid going back through the discretionary review process, the project should likely be conditioned such that the fenestration is done to a suitable model, and allow staff to verify this. Otherwise, the project would likely have to be deferred depending upon how much fenestration is being requested.

Commissioner Mulheren stated the applicants have done a nice job redesigning the project, as recommended by the Planning Commission that reviewed the initial project. He supports if windows are added to a particular elevation, for instance, the applicants/architect should be allowed to exercise discretion as to the quantity and placement.

Commissioner Molgaard supports elevation changes incorporating a treatment to the wall for the drive-thru pharmacy because it will be seen by people.

The Commission addressed which elevations would be the most appropriate for windows.

Jason Brennen referred to the building elevation relative to the site plan A-2.1, and identified which elevations have windows proposed, including the design techniques.

ON A MOTION by Commissioner Molgaard, seconded by Commissioner Landis, it was carried by an all AYE voice vote of the Commissioners present to approve Mitigated Negative Declaration for Major Site Development Permit No. 06-19/Minor Subdivision Map No. 06-18, as discussed above and as outlined in the staff report.

ON A MOTION by Commissioner Molgaard, seconded by Commissioner Anderson, it was carried by an all AYE voice vote of the Commissioners present to approve Minor Subdivision Map No. 06-18 with Findings 1-6 and Conditions of Approval 1-7, as discussed above and as outlined in the staff report.

Staff noted the recommended parking Condition of Approval No. 48(b) sufficiently addresses the bicycle rack issue.

The Commission recommended modification to Condition of Approval No. 48(b) to read, "The provision of bicycle parking for a minimum of six customers in a paved area near the entrance of the building and for three employees within the building or in a secure area outside the building with weather coverings."

The Commission recommended Condition of Approval No. 10 regarding signage is modified to read, "The number and size of freestanding and building signs shall be restricted to those prescribed by the Sign Code provisions of the Ukiah Municipal Code or permitted in a final sign program approved by the Ukiah Planning Commission as part of the discretionary permit approval for the project. The project site shall be limited to a maximum of one freestanding sign on a monument base with a maximum height of eight feet for the sign copy and ten feet for the architecture on top of the sign." Staff noted the matter of signage is addressed in the Site Development Permit conditions, as well as the Mitigated Negative Declaration as a mitigation measure under aesthetics.

It was noted the Lighting Plan is addressed in Condition of Approval No. 11 with the addition of one sentence to read, "The light poles used in outdoor areas shall be decorative in design, with features that are compatible with the design of the building." Also, the final Lighting Plan shall be reviewed by the Planning Director should be included in this condition.

The Commission recommended adding subsection "I" to Condition of Approval No. 49 relevant to the Landscaping Plan to read, "A reduction in the number of parking stalls to 58 and the use of the area for increased landscaping along the Gobbi Street frontage."

Staff confirmed as part of the Landscaping Plan a report must be made concerning the health and feasibility of retaining the existing trees located in the planter area abutting South State Street and that the trees proposed for the parking lot when mature are required to provide a 50 percent shaded area coverage. This condition is particularly pertinent along the south and west sides of the parking lot which receives considerable exposure from the sun and therefore, the Final Landscaping Plan compels the submittal of a Shading Plan prepared by a landscape professional that shows the areas of the parking that will be shaded by the proposed shade trees when mature. The Building Permit cannot be issued until this provision of the Landscape Plan is complete.

Commissioner Molgaard addressed the issue of paving materials and inquired whether there is a way to more thoroughly investigate the use of permeable materials versus asphalt for the parking lot in terms of durability and feasibility.

Associate Planner Lohse stated the City Public Works Department will be attending a seminar on permeable paving. Permeable paving are typically approved for residential driveways. Staff has looked at variations of permeable paving techniques that include packed gravel for projects. The concern is that permeable paving materials do not hold up over a long period of time in parking lots where continuing maintenance would be required. However, permeable paving is becoming more viable as continued research is made in this regard. He acknowledged the Commission supports the use of permeable paving near tree wells for this project. The use of permeable paving may be acceptable for the parking stalls, but not for the driveway and/or driveway apron areas where asphalt would likely have to be used.

Chair Pruden stated it is unlikely that Condition of Approval No. 46 that addresses paving can be modified.

Jason Brenner does not support that the applicants should be required to use permeable paving substances in high traffic areas, particularly when the durability and feasibility have not been effectively tested or proven. After a period of time and exposure to the elements of rain, dust, and other factors the material becomes ineffective where oil, grease, and gas go straight through the material and into the ground and the original function lost. He suggested additional research be conducted concerning the use of permeable paving materials for parking lots.

The Commission discussed the matter of excess water runoff from commercial parking lots that use asphalt and the potential impacts to the environment from vehicle contaminants mixed with excess storm water runoff.

Dan Taylor, applicants' representative, commented on the issue of permeability and paving materials. It is his understanding from resource materials that the only effective way to deal with pollutants that get into the ground water and eventually into the creeks and streams is to treat it through a bio-filtration swale from plant material that thrives on absorbing hydrocarbon contamination. After the water is decontaminated, it can be released into the storm drain system. The permeable paving materials eventually become impervious so it is not commonly use in a commercial application.

Commissioner Landis recommended a small portion of the site be paved with permeable materials where feasible to demonstrate that the materials do or do not work, as a compromise.

Dan Taylor stated Rite Aid may amenable to utilizing a portion of the site as a demonstration area to test the permeability of the materials and whether there is a beneficial effect on the trees as opposed to a storm water treatment facility, provided the City Engineer determines this is acceptable. He recommended using the employee parking stalls and planter in this area for this purpose. Use of permeable materials would not be feasible for high traffic areas or in areas where there is heavy truck traffic.

Chair Pruden recommended that Condition of Approval No. 46 include the use of permeable paving materials "where feasible."

Associate Planner Lohse stated Condition of Approval No. 46 could be modified to reflect that permeable paving may be permitted for low traffic/truck areas if the City Engineer/Director of Public Works determines it will maintain its structural integrity.

Chair Pruden stated Condition of Approval No. 46 should state permeable material be allowed where feasible with approval by the Director of Public Works.

Jason Brenner stated more information and knowledge of the technology is necessary before Rite Aid is required to use permeable paving materials. He stated the City could conduct a demonstration concerning the use of permeable paving materials.

Associate Planner Lohse stated staff is investigating the application of permeable paving materials.

Commissioner Landis supports investigating the use of permeable paving materials for the good of the community and environment. She thanked the applicants for allowing a permeable paving demonstration area.

Commissioner Molgaard asked about the use of energy efficient for the project.

Dan Taylor stated windows are essentially a waste of energy. There is no window that is as efficient as an R-19 wall. California State law under Title 24 limits how many square feet of windows a structure can have based on the square footage of the building. Most of the project's energy conservation measures amount to fewer windows.

Commissioner Molgaard commented if the walls included other architectural features, the fact that there are no windows would not be evident. Two primary issues that should be carefully considered for projects are energy efficiency and aesthetics.

Commissioner Molgaard understands that even though Title 24 is important in California, the intent is to encourage the community to think beyond the minimum elements required for energy efficiency/conservation.

ON A MOTION by Commissioner Anderson, seconded by Commissioner Landis, it was carried by an all AYE voice vote of the Commissioners present to approve Major Site Development Permit No. 06-19 with Findings 1-12 and Conditions of Approval 1-55 with modifications to Conditions of Approval Nos. 10, 11, 44, 46, 48, and 49, as discussed above and as outlined in the staff report.

Break: 8:33 p.m.

Reconvene: 8:48 p.m.

~~9C. Zone Change No. 05-24 and Minor Subdivision Map No. 05-25, as submitted by Ruff & Associates to develop the Orrs Creek Neighborhood Development. Specific project requests include rezoning the site from R-3 (High Density Residential) to PD (Planned Development), dividing the parcels comprising the 1.49-acre site into 4 lots and a remainder lot, and establishing site-specific planned development standards that would allow the construction of a town home and a second dwelling unit on each of the four new lots. The residential building on the remainder lot would be retained as part of this project. The project is located at 125-137 Ford Street (APN 002-121-02, 08, & 11), approximately 450 feet south of its intersection with State Street.~~

~~Associate Planner Lohse reported on the staff report as follows:~~

- ~~• The project involves a rezone and minor subdivision into four new lots and a remainder lot on Lot 1 relative to the initial 2005 subdivision of a parcel into three lots. The proposed rezone establishes a residential planned development for the property. Lots 1-4 will be developed with residential uses and structures associated with parking and landscaping.~~
- ~~• Specifically, each of the new lots would be developed with a townhome and a second unit with a common recreation area on the rear portion of the proposed Lot 1 and the existing residence on the remainder lot would be retained and remodeled for continued residential use.~~
- ~~• Planned Development projects require City Council approval.~~



City of Ukiah

STAFF REPORT TO THE PLANNING COMMISSION

RITE-AID DRUGSTORE Minor Subdivision Map 06-18 & Site Development Permit 06-19

ITEM NO. 9-B

Meeting Date: February 14, 2007

PROJECT SUMMARY

The approval of the proposed Minor Subdivision Map (MS 06-18) will allow the division of the existing 2.5-acre subject property into proposed Parcel One, with 68,488 square feet, and Parcel Two, with 38,001 square feet. The existing Rite-Aid store would be located on Parcel Two, with the proposed boundary line running in a north-south direction approximately 5-10 feet east of its eastern wall.

The approval of the Site Development Permit (SDP 06-19) would allow the construction of a new 17,422 square foot drugstore building in the center of proposed Parcel One. This building would be used as a new Rite-Aid store that would replace the existing store. The existing building would be demolished prior to the recordation of the subdivision map and/or the occupation of the new store, with no development planned for Parcel Two.

PROJECT RECOMMENDATION

Planning Department staff recommends that the Planning Commission takes the following actions:

1. Adopt the Mitigated Negative Declaration/Initial Study prepared for Minor Subdivision Map No. 06-18 and Site Development Permit No. 06-19, including the recommended Mitigation Measures and Mitigation Monitoring Plan;
2. Approve Minor Subdivision Map No. 06-18, based on recommended Findings and subject to Conditions, and
3. Approve Minor Subdivision Map No. 06-18, based on recommended Findings and subject to Conditions.

PROJECT LOCATION

The subject property consists of a single parcel located at 680 South State Street (APN 002-301-43), on the northwest corner of the intersection of State Street with Gobbi Street. This large lot also has frontage along South Oak Street.

GENERAL PLAN DESIGNATION: C (Commercial)

ZONING DISTRICT: C-1 (Community Commercial)

ENVIRONMENTAL DETERMINATION

The Environmental Coordinator determined that the proposed retail/subdivision project is *not* exempt from the provisions of the California Environmental Quality Act (CEQA). As a result of this determination, Planning Department staff prepared an Initial Study in which it determined the proposed multi-family housing project would cause potentially significant adverse environmental effects related to Aesthetics; Air Quality; Cultural Resources; Geology/Soils; Hazards/Hazardous Materials; Hydrology/Water Quality; and Public Utilities. However, the environmental study also includes specific measures designed to mitigate the effects cited above to levels that would be less than significant and these measures have been incorporated into a Mitigation Monitoring Program for implementation prior to or during construction phases of the proposed development. They are also included as recommended Conditions of Approval Numbers 9-35 for this project. Based on these measures, a Mitigated Negative Declaration was prepared for the project and a copy of this document is attached to this staff report.

PROJECT DESCRIPTION

Existing Conditions

The subject property consists of a single 2.5-acre parcel that extends from the west side of South State Street to the east side of South Oak Street, with a third frontage along West Gobbi Street. The lot is fully developed with the existing 31,500 square foot Rite-Aid drugstore, with the west-facing wall of the retail store abutting the sidewalk for South Oak Street and its customer entrance facing the large asphalt parking lot that covers the eastern portion of the site. This store is approximately 18 feet above grade, with a flat roof over most of the structure and a mansard roof cover over the customer entrance area, which consists of glass entry doors and large-pane windows. Walls are solid masonry inset with rock, with no windows or customer entrances on the north, west, or south sides.

The eastern portion of the site is developed with a large parking area that has approximately 154 parking stalls, with perimeter landscape planters along the eastern and southern sides. These planters do not have extensive landscaping and there are only a few shade trees in smaller planters at the ends of long rows of parking. State Street access is available via a 36-foot wide driveway at the northeast corner of the lot, with additional available via a 31-foot wide driveway near the center of Gobbi Street frontage and a 30-foot wide driveway along Oak Street, near its intersection with Gobbi Street. The area south of the building is used for truck access to the loading dock on the southwest corner of the building.

As noted above, the project site has been almost entirely developed, with building foundations or asphalt parking/access roadways covering at least 90-95 percent of its surface area. This area is relatively flat, with drainage running from west to east.

Proposed Conditions

The applicants have filed two applications to develop the new Rite-Aid drugstore, as described below.

- ❖ ***Minor Subdivision Map No. 06-18:*** The approval of this application would divide the 2.5-acre parcels into proposed Parcel One of 68,488 square feet and Parcel Two with 38,001 square feet, respectively. The existing Rite-Aid store would be located on Parcel Two, with the proposed boundary line running in a north-south direction approximately 5-10 feet east of its eastern wall. The new store would be constructed on Parcel One, which consists of the existing parking lot.
- ❖ ***Major Site Development No. 06-19:*** The approval of this project component would allow the actual development of the proposed retail drugstore on the eastern portion of the site. This new commercial development will include the specific design features outlined below.

- The construction of a 17,422 square foot drugstore building in the center of proposed Parcel One. This building would parallel the State Street frontage, with its east-facing wall approximately 80 feet from the back edge of the sidewalk that fronts this street and the south side of the building 50-70 feet from the Gobbi Street right-of-way.

This building would be 23-25 feet high, with a flat roof obscured by a segmented parapet roof treatment along all 4 sides. The building would be generally rectangular in shape, with the southeast corner cut off at an angle to provide a glassed-in corner entry for customers. There would also be large pane windows on the south and east walls, abutting the corner entry, and smaller-pane windows along near the middle part of the east wall and the west wall. There would also be smaller pane windows on the "drive-thru pharmacy" proposed for the northern wall of the building.

Building walls would be constructed with concrete masonry blocks covered by an attractive mix of masonry and metal materials. These include outset walls of "bronze-tone" brick on either side of the customer entry area and on the other corners of the building. The inset walls between the brick columns would consist of lighter "golden-buff" brick approximately 13.5 feet from grade, with white scored stucco covering the walls above this brick. The walls would also utilize cast-stone moldings on the bricked areas and stucco covered foam trim on the stucco walls. Additional wall accents would include white metal trellises on the inset walls on the building's east and west sides, and a bright blue metal panel over the building entrance.

The north side of the building would also have a 23-foot wide by 30-foot long canopy roof over the pharmacy drive-thru area. This flat-roofed structure would be constructed with masonry materials covered by white and blue metal panels similar to those used on the building's front.

- The areas surrounding the building would be used for paved access and parking or landscaping. The project site would utilize the same driveway locations as the existing store, but the driveways along State Street and Gobbi Street frontages would both be reduced to 30-foot widths. These driveways would provide ingress and egress to access lanes that will circle the building, providing through access to persons using the drive-thru pharmacy and to persons who park on the site.

The most accessible parking will be in two rows of stalls along the east and south sides of the building, with 6 compact and 9 full-size stalls to the south and 3 handicapped parking stalls and 26 full-size stalls to the east. There are also 17 full-size stalls proposed for the west side of the building, but these stalls are not as accessible to the store entrance and will require users to walk through the loading dock area that would be developed on the west side of the store. The area west of the store would also be used for a 430 square foot trash/recycle area that would be constructed with masonry materials similar to the building.

- The remaining areas of the site will be used to widen the existing linear planter areas along the east and south perimeters of the site and add new linear planters along the western perimeter. The existing trees fronting State Street will be removed for aesthetic reasons, but will be replaced with ornamental trees that will shade both the parking lot and the abutting sidewalk. Smaller planter areas in the northeast and northwest corners of the site will also be added, with 13 shade trees planted in wells between parking stalls.

The conceptual landscape plans submitted with the project shows that the landscaped areas would be planted with groundcover, shrubs, lawn, and trees, including deciduous Maple tree species (*Acer Rubrum* October Glory) and European Hornbeam species (*Carpinus betulus*) in the perimeter planters along the east and south sides of the building and in the planters located in the parking areas. Three evergreen tree species (*Cuppiessus semervirens* 'glauca') would be planted in the planter area proposed for the southwest corner of proposed Parcel One.

The landscape plan also includes dedicated pedestrian paths between the store entrance and the intersecting sidewalks along State and Gobbi Streets, with an unspecified paving that would be different than asphalt.

Staff notes that the proposal for this project entails the demolition of the existing Rite-Aid drugstore building, which will be necessary to comply with a variety of zoning and subdivision ordinance standards. The applicants have submitted a plan for the demolition to occur shortly after the proposed drugstore opens for business.

Surrounding Land Uses and Setting

The subject properties are located in an urbanized area of Ukiah that has been developed with retail and professional offices to the south, the Safeway supermarket to the east, a motel to the north and low- to high-density residential to the west.

STAFF ANALYSIS

General Plan Compliance

The Ukiah General Plan Land Use Map classifies the subject property for C (Commercial) land uses and staff has determined that the proposed subdivision of the site and its subsequent development with a retail store/pharmacy is consistent with the list of allowable land uses for this classification. The project design is also consistent with the Land Use Element requirements for the C classification, including those for new development. These include siting criteria for location in commercial land use areas, adherence to applicable zoning standards, access on or near public streets, internal access onto and within the project site, and building intensities. The proposed site layout, as conditioned, is also consistent with special development issues, including landscaping, on-site parking, lighting and glare, and conformance to noise standards.

Staff also noted that the site is located in an area that is defined in the Community Design Element as part of the South State "gateway" where people's opinions on the appearance of the City can be formed. In fact, the project area is located near the southern end of what many residents think of as "downtown Ukiah", and higher design standards are encouraged to ensure compliance with the intent of this element. In this case, the proposed building design is a fairly standardized model that has been made more unique in appearance by the addition of off-set walls, varied roof lines and treatments, differentiated brick masonry and stucco wall coverings, and varied wall colors. The project also utilizes a more effective site layout and more intensive landscaping along site perimeters. Based on these building and site design features, it is the opinion of staff that the proposed project presents a more interesting and attractive appearance than the existing development, and that these features provide an adequate level of consistency with applicable design guidelines. Therefore, it is staff's opinion that the project is consistent with surrounding development and the intent of the Community Design Element.

Staff also reviewed the project's consistency with the other elements of the General Plan and found no inconsistencies with any of these goals and policies. Based on this determination and the consistency with the elements discussed above, it is the opinion of Planning Department staff that, overall, the proposed drugstore development is consistent with the Ukiah General Plan.

Consistency with Zoning Standards

The subject property is located in the C-1 (Community Commercial) Zoning District and the proposed land division and drugstore development are subject to the use requirements and development standards for this district. In this case, the retail drugstore use is allowed, but the development of the 17,422 square foot store building does require the approval of the proposed site development permit.

After review of the applicable C-1 development standards, staff has determined that the proposed lots are consistent with the building site area requirements for corner lots, which require at least 7,000 square feet of lot area for each parcel and at least 70 feet of lot width.

Once the lots are divided and the new drugstore is constructed, the applicants intend to demolish the existing structure to ensure consistency with lot coverage requirements. At this point, there is no specific proposal for new development on this area and staff's analysis is, therefore, confined to the new drugstore development's consistency with the C-1 development standards. Specific analyses are included below.

Building Height: The proposed single-story drugstore structure will be approximately 26 feet high, which is roughly half of the maximum building height of 50 feet for the C-1 zone. This structure height is also consistent with commercial buildings on abutting lots.

Setbacks: The proposed single-story structure will be set back 55-75 from the Gobbi Street frontage and at least 75 feet from the State Street frontages, so it will comply fully with the C-1 setback standard of 10 feet for a single-story structure. Proposed Parcel One would abut commercially zoned lots on all sides and is not, therefore, required to have any side or rear yard setbacks. However, staff notes that the centrally located building will be at least 25-85 feet from the proposed interior boundaries, providing more than adequate area between the structure and other existing structures.

Parking: The retail store proposed in this project is required to provide one parking stall for every 300 square feet of gross floor area, which equates to a minimum of 58 parking stalls for the project. Project plans show that the site will have at least 61 parking stalls in rows of parking along the east, south, and west sides of the building. Planning staff finds that this parking ratio is adequate and would, in fact, support the removal of the three extra stalls to allow the expansion of the perimeter landscaping strip proposed along the southern side of the lot. This recommendation is discussed more thoroughly in the landscape analysis below.

While the number of stalls is adequate, staff doubts that the parking areas on the west side of the building will be used regularly by customers due to their distance from the building front entrance. However, if these stalls are reserved for and used by employees, the stalls closest to the customer entrance will be more accessible for customers. Therefore, staff recommends that the parking lot design is approved, but that 5-7 of the parking stalls located furthest from the building entrance, in the westerly row of parking, are clearly designated for employee usage.

Staff notes that project plans also show that a bike rack will be installed in the planter area proposed near the building's entrance. The plans do not show how many bicycle parking spaces will be located in this rack, but the number of bike parking spaces required in the C-1 zone equates to at least 10 percent of the number of required automobile parking stalls, or at least 6 bicycle spaces for this project. In this case, staff recommends that the proposed bike rack is taken out of the planter area and placed on a paved and covered area that will encourage its use throughout the year. Staff also recommends that the applicants design a secure bike parking area for employees, preferably within the actual building or some other weather-resistant area.

Based on the potential for reducing the number of vehicle parking stalls and the need for ensuring that the site has at least 6 bicycle parking stalls, staff will recommend that a final parking plan is prepared for the site, with final review and approval of the design designated to the Director of Planning and Community Development Department. This will allow for some flexibility in modifying parking lot/bike parking design without formal amendments to any plan approved by the Planning Commission.

Maximum Lot Coverage: The 17,422 square foot store building will cover approximately 26 percent of the 68,488 square foot area for proposed Parcel One, which is well within the maximum lot coverage ratio of 40 percent.

Landscaping/Lighting: The conceptual landscape plan for the project utilizes perimeter planters and planters abutting the proposed structure to provide approximately 9,000 square feet of landscaped area, with the majority of these areas devoted to live plantings. Specific plants proposed include 3 different deciduous and evergreen tree species, 9 different shrubs, and small patches of lawn.

The landscape criteria for the C-1 zone encourage the retention of existing vegetation where feasible. In this case, all existing trees are proposed for removal, including the line of non-native trees located in the planter strip that runs along the east side of the property. These trees have been poorly maintained, but do not appear diseased or in distress, and it is staff's opinion that their retention as part of the landscape plan should be given serious consideration. The applicants indicate that their proposed replacement is based primarily on aesthetic reasons, but staff recommends that they only be removed if a certified arborist or other landscape professional verifies that the previous poor maintenance has injured the trees or otherwise stunted their growth and potential as an effective shade tree for the site. This recommendation is included in recommended Condition No. 49.

The C-1 zone landscape criteria also requires that all landscaping is proportional to the building elevations, utilizes native plants or plants that grow well in Ukiah's climate without extensive irrigation, and is of sufficient size, health and intensity so that a viable and mature appearance can be attained within a reasonably short period of time. None of the plants proposed in this project are native species, but all are expected to grow well in Ukiah' Mediterranean climate and will be proportional with the height of the proposed building. The trees proposed in the project are expected to take 10-20 years to reach full height and canopy widths, but the 15-gallon sizes proposed for all of the trees should provide reasonably tall canopies within a much shorter period of time.

The C-1 landscaping criteria also require that deciduous trees are planted along the south and west building exposures to provide shade. In this case, there will be three deciduous trees in the planter proposed along the Gobbi Street frontage and three abutting the south side of the building, which appears to be consistent with the criteria.

The only trees proposed for the west side of the building are 4 smaller ornamental deciduous trees in the parking area, which are not expected to provide much shade for the building, but staff notes that this area is devoted primarily to access driveways and the building's loading dock area. Based on this design, it is staff's opinion that the placement of additional shade trees in this area is not critical to the overall effectiveness of the site's landscape plan and should be permitted as a minor modification.

The landscaping criteria also require that the parking lot for this project has a tree placed between every 4 parking stalls within a continuous linear planting strip rather than within individual planting wells, and that the placement of the trees provide a canopy covering at least 50 percent of the parking lot within 20 years. In this case, the planting plan includes 4 deciduous trees in a continuous linear planting strip along the State Street frontage, but the remaining 13 trees proposed for shading the parking lot are planted in individual tree wells. These trees consist of smaller ornamental trees that do not typically require as much planting area, but staff recommends that their inclusion in the approved landscaping plan should be subject to verification of their growth potential by a landscape professional or certified arborist. This recommendation is included in recommended Condition No. 49.

No specific details on shading patterns or coverage were included in the conceptual landscape plan, so there is no way to determine whether the project will comply fully with the C-1 requirements that trees provide shade over 50 percent of the parking lot areas within 20 years of their being planted. It is staff's opinion that the trees proposed for east of the building will provide the required coverage, but the more sparse tree plantings along the west and south sides of the building are not expected to provide adequate area. Based on these concerns, staff recommends that the applicants are required to provide a shade canopy analysis prepared by a landscape professional as part of the final landscape plan, and that additional trees are planted prior to the occupancy of the building if the study determines they are needed to comply with the shade canopy requirements. This recommendation is also included in recommended Condition No. 49.

The C-1 landscape criteria also require the submittal of a lighting plan. In this case, project plans show at least 10 exterior lights on poles ringing the parking lots on the east, south, and west sides of the building, with some additional security lighting on the building. However, the plans do not include details on the design of the lighting supports or the areas that will be lit by these lights and staff recommends that such details be required as part of a final lighting plan to be reviewed and approved by the Director of Planning and Community Development prior to the occupation of the building. Staff further recommends that the lighting plan use lighting poles with designs and heights that are compatible with the design and height of the proposed drugstore structure and consistent with accepted "dark sky" guidelines for keeping light downcast and on the subject property. These recommendations are included as recommended Condition No. 11.

Consistency with Commercial Design Guidelines

The project site is located within the Downtown Design District for the City of Ukiah and the development of the proposed drugstore is subject to the Design Guidelines for Commercial Development in the City of Ukiah, which was adopted by the Planning Commission and City Council in 2001. In order to assess the project's consistency with these guidelines, staff examined a broad range of topics for which brief analyses were prepared.

Site Planning

Site Features: According to site planning guidelines, site features such as trees, lot size and shape, and relationship to surrounding development should be a compelling consideration in site design. In this case, the largely developed site contains few natural amenities other than the row of trees along State Street, which would be removed by the applicant and replaced with a tree species that is considered to be more attractive. Staff notes that the existing trees appear to be poorly maintained and do not exhibit a well-defined canopy at this time, and it is likely that their replacement with new trees will eventually contribute in a positive manner toward the overall aesthetic improvement of the property. However, staff also notes that the replacement of the existing trees for aesthetic reasons is considered a subjective decision with which many persons will disagree and has recommended that their suitability for retention be examined as part of the final landscape plan.

In terms of the overall site layout, it is staff's opinion that the proposed location of the drugstore structure will be highly compatible with surrounding development, which includes commercial and high-density structures with one-story to two-story heights. In addition, both the commercial building and ancillary structures, such as the pharmacy window drive-thru, loading dock, and trash/recycle area, will be far enough from abutting development to ensure that they do not conflict with each other. Based on these factors, it is staff's opinion that the proposed development does not conflict in a substantial way with existing site features, and will effectively, provide a more attractive site characteristic than the existing development on the site does.

Parking: The site planning guidelines also recommend aesthetic screening and shading of parking, clear designation of parking areas, dispersing parking stalls where feasible, and the development of safe and well-marked pedestrian walkways. The parking for the proposed project will be in long rows of parking located along the east and south sides of the building and along the site's eastern, western, and southern perimeter areas. All of these parking rows will be broken up by landscape planters with small ornamental trees installed between every 4 or 5 parking stalls. This parking lot design does not fully accomplish the smaller pockets of parking stressed in the guidelines, but the inclusion of regularly distributed tree wells and vegetation does disperse the trees in a manner that provides adequate compliance with the parking lot guidelines.

In addition, the parking lot provides a very effective pedestrian crossing between the store entrance and the intersection of Gobbi Street with State Street. Based on these factors, it is the staff's opinion that the parking guidelines have been met.

Pedestrian Orientation: The site planning guidelines also recommend an emphasis on pedestrian orientation. The project site will be served by 5-9 foot wide sidewalks that run along Gobbi Street and State Street, with pedestrian pathways providing dedicated ingress and egress to the store's main entrance and to the 4-foot wide sidewalks that abut the east and west sides of the building.

Staff is concerned that the placement of parking stalls along the site's western perimeter will require customers to walk through the paved area that is intended to serve as the access driveway for the drive-thru pharmacy and the building's loading dock. However, Planning staff has recommended that the northernmost stalls in this row of parking be reserved for employees who will be aware of the on-site traffic that will flow through this area, while the rest of the stalls are far enough south of the pharmacy drive-thru window and loading dock that south-bound traffic will be visible. Therefore, it is the opinion of staff that this project is highly walkable and complies with this guideline.

Compatibility: The site planning guidelines recommend that development is compatible with surrounding land uses from both a functional and aesthetic standpoint. In this case, the new drugstore building is considered highly compatible with the architecture on the surrounding commercial structures, particularly with the inclusion of the varied and interesting design features outlined in the project description above. In fact, staff considers the proposed structure design to be much more attractive than that used on the existing drugstore building, and in some cases, with other surrounding structures.

The compatibility guidelines also recommend that development should not create unattractive views for neighbors or traffic corridors, that all exposed elevations should maintain consistent architectural character, and service areas and utility equipment should be screened from view. In this case, the building will be the most visible to persons using the State Street and Gobbi Street corridors, but as staff has pointed out throughout its analysis, the building is designed with a high degree of architectural compatibility to surrounding buildings and is often more attractive than the existing structures. Therefore, it is the opinion of staff that this building will be highly compatible with the surrounding development.

Coordination with Adjacent Properties: The guidelines encourage property owners to develop shared facilities, such as driveways, parking areas, pedestrian plazas, and walkways to maximize usable areas. In this case, such coordination is not possible since the buildings on the lot north of the site are too close to the common property line to allow joint driveway development or shared ingress and egress. It may be possible to do shared access with the future development that occurs on proposed Parcel Two, but it is not feasible at this time since there is no specific development project is proposed at this time.

Setbacks: The guidelines recommend putting commercial buildings as close to the right-of-way as feasible to present a strong architectural presence and more intimate visual scale along storefronts. The proposed drugstore building faces both State Street and Gobbi Street, but would be between 55-80 feet from the rights-of-way for these streets to accommodate internal circulation and parking. The applicants have also included perimeter planting areas to break up the views of the paved access lanes and

parking, but the distance between the public sidewalks and the building is too great to achieve either a strong architectural presence or an intimate visual scale. Based on these factors, it is staff's opinion that the proposed site design is not consistent with the intent of this guideline, although it is somewhat consistent with surrounding commercial development.

Building Materials: The guidelines recommend using masonry materials as the primary building material for buildings in the downtown core. The proposed drugstore building will be constructed with masonry block walls covered with a variety of different colored bricks and stucco that will contribute to the unique visual qualities of the building. Therefore, it is staff's opinion that the proposed building materials comply with the intent of this guideline.

Color: The guidelines do not authorize the Planning Commission to approve or disapprove building colors, but encourages the applicants to use colors that relate to natural building materials and/or are compatible with surrounding development, and to use coordinated colors when multiple storefronts share a common façade. The proposed drugstore building will use a variety of brick wall coverings that are earth-tone in color, with darker browns to lighter and brighter tans distributed throughout. Lighter stucco near the tops of some of these walls will be added to provide contrast, but the only bright colors will occur on the bright blue canopy over the store entrance and in the red, white, and blue signs on the east and south sides of the drugstore structure. Overall, staff finds this color scheme to be consistent with the guidelines.

Roof Facades & Store Front Openings

Roofs & Rooflines: The guidelines recommend that the form, color, and texture of the roof should be considered an integral part of the building design and compared to rooflines on nearby structures. They also recommend that diversity in parapet and roof shape be included to add interest to the building or rhythm of existing storefronts. The drugstore building will have a flat roof, but it will be effectively screened by a variegated parapet wall that will also contribute to the architectural articulation of the building and break up the building mass on the longer walls. It is staff's opinion that this design will provide an interesting and attractive appearance that is consistent with the intent of this guideline.

Facades & Store Front Openings: The guidelines recommend that new facades utilize cornice lines, parapets, eaves, awnings, windows, balconies, entry insets and signs to articulate a building front. In this case, the building design precludes some of the measures listed, such as the balconies and eaves. However, the building walls will have offset planes, variegated parapet roofing, wall trellises, windows, and a canopy roof over the store entrance to break up wall mass and provide interesting variation. In addition, the building materials on these walls will provide even more articulation to the wall facades, resulting in an effective and attractive commercial building façade that is compatible with the design features found on nearby commercial buildings. Therefore, it is the opinion of staff that the proposed facades along the State Street and Gobbi Street frontages are consistent with these guidelines.

Window Openings: The guidelines recommend that window space along a commercial building's frontage should be designed to provide a maximum amount of retail exposure with entryway insets to help break up the building's mass. The windows on the east and south sides of the mixed-use structures vary in size, ranging from the larger panes near the building's entrance to the smaller window treatments beneath the proposed trellises. The northern and western sides of the building do not have any substantial window treatments, but these elevations do not face the abutting streets. Therefore, it is staff's opinion that the proposed window treatments are generally consistent with the guidelines.

Rear Entries: The guidelines recommend that rear entrances be equally as attractive as the front entrances. In this case, the only customer entrance is on the southeast corner of the building; all other entrances are for the loading dock area or employee-only doors on the west side of the building.

Sign Design: The applicants have submitted a conceptual sign program for the new store and staff has determined that this plan contains well-designed signage that is generally consistent with the sign code provisions of the Ukiah Municipal Code and, in fact, contains much less signage overall than allowed for this site. However, staff noted the program shown on plans includes the retention of an existing pole sign and the proposed use of additional monument signs along both the State Street and Gobbi Street frontages, resulting in three freestanding signs on the site. The UMC clearly limits commercial developments to a single freestanding sign and the construction of two new monument signs would require the approval of a variance that staff is not prepared to support. After discussions with the applicants, they have indicated that they will remove the existing pole sign and replace it with a single freestanding sign on a monument base that will be located in the planter area proposed for the southeast corner of the site.

The conceptual design for the proposed monument sign, which is attached to this report, shows that it would be mounted on a small brick-covered base, with two white-colored aluminum columns supporting the 5-foot high by 10-foot wide sign. The top of the sign would be 13-14 feet above grade, but the columns would also support a trellis-like top that would be approximately 16.5 feet high. This sign is substantially lower than the existing pole sign on the site, and is comparable in height to existing poles signs on other nearby commercial lots. However, it is also nearly twice the 8-foot height approved most recently for signs along the State Street corridor and it is staff's opinion that the height of the sign cabinet should be reduced to 8-10 feet to be more consistent with these signs, and that the architectural trellis feature should be reduced to a maximum of two feet above the top of the sign cabinet. This would bring the maximum height of the sign poles to between 10-12 feet above grade.

Staff reviewed the building signs that would be limited to the east and south faces of the building and found them to be consistent with Sign Code provisions and compatible with the building signs located on nearby commercial buildings.

Pedestrian Circulation and Lighting: These guidelines recommend that clearly marked and well-lit pedestrian access between parking lots and the commercial building should be provided, along with bicycle parking. The proposed parking lot has excellent pedestrian access to the customer entrance and to the sidewalks proposed for the east and south sides of the proposed drugstore building.

The project designs also provide a single bike rack that will be installed in the landscape planter near the building's entrance; this bike rack provides sufficient consistency with the guidelines, although staff is recommending that it be moved to a more accessible area with pavement or walking stones, and if feasible, is covered by a roof or canopy that will encourage year-round use.

Landscape Design: The guidelines recommend the use of plants adapted to Ukiah's climate, plants that are proportional to the buildings on the site, the use of color spots and annual plantings, and 20 percent coverage in parking areas. As noted in the landscape plan discussion of the C-1 zoning analysis above, the proposed landscaping has a fairly comprehensive landscape plan that generally provides most of the desired landscaping. Some modifications have been recommended by staff, but overall, in their opinion that the landscape plan provides a high degree of consistency with the landscaping recommended in the design guidelines.

Consistency with Airport Compatibility Criteria

The project site is located approximately ¾ of a mile NW of the Ukiah Municipal Airport. The subject parcel lies within Compatibility Zone C, as defined in both the Ukiah Municipal Airport Master Plan and the County of Mendocino's Airport Comprehensive Land Use Plan (CLUP). According to the compatibility tables listed in these plans, Compatibility Zone C is an area which is characterized as having *limited risk with frequent noise intrusion*, but it includes intensive retail land uses such as the proposed pharmacy as a *normally acceptable use* within this zone so long as the development complies with certain criteria. These consist of human use densities of 150 persons per acre, 15 percent of lands remaining in open space, and a limitation of 15 units per acre. Staff reviewed the potential land uses and determined that project does meet all of these criteria, and is therefore, consistent with the airport master plans.

Consistency with Ukiah Subdivision Ordinance Standards

The proposed subdivision map would divide a 2.5-acre lot that is commercially zoned and developed with a large commercial building on its west perimeter. Therefore, staff reviewed the subdivision provisions for Type VI subdivisions (non-residential land uses), as defined in the Ukiah Subdivision Ordinance. Based on this review, Planning Department staff determined both of the proposed lots would be consistent with standards for the development of street improvements, the provision of water supplies, fire protection, sewer hook-ups, storm drainage, utility easements, and street lighting facilities. Therefore, it is staff's opinion the proposed subdivision will be consistent with the Subdivision Ordinance.

This determination, however, is based on the timely demolition and removal of the existing Rite-Aid building since this large structure would cover almost all of the area proposed in Parcel Two. Conditions requiring this demolition are included below.

CONCLUSIONS: After extensive environmental and planning-related reviews of this project, it is the conclusion of staff that the proposed drugstore development will utilize building and site designs that are attractive enough to meet the multiple and varied design standards and guidelines that apply to the project area.

Staff also concludes that the proposed project is consistent with applicable goals and policies of the Ukiah General Plan, Ukiah Municipal Code standards for commercial site development, the standards of the Ukiah Subdivision Ordinance, and the criteria of the Ukiah Municipal Airport's C Compatibility Zone.

FINDINGS for Minor Subdivision Map No. 06-18: The Planning Commission's recommendation for the approval of Minor Subdivision Map No. 06-18 is based, in part, on the following findings:

1. The parcels established by the Minor Subdivision, as conditioned, are consistent with the requirements of the California Subdivision Map Act;
2. The parcels established by the division are consistent with the Ukiah General Plan requirements for parcel size and new development, as defined in the Land Use Element of this Plan;
3. The parcels established by the division are consistent with the applicable use and development standards for the C-1 (Community Commercial) zoning district;
4. The parcels established by the division are consistent with the Ukiah Subdivision Ordinance since both lots conform to the standards for Type VI subdivisions, including those for lot areas, lot width, street improvements, water supply, fire protection, and other standards;
5. The parcels established by the division are consistent with the density, open space, and infill criteria for the C Compatibility Zone of the Ukiah Municipal Airport Master Plan and the Mendocino County Airport Land Use Plan; and
6. A Mitigated Negative Declaration prepared for the Minor Subdivision Map (MS 06-18) and a Site Development Permit (SDP 06-18) to allow the subsequent development of one of the lots established by the subdivision determined that the project would cause potentially significant adverse environmental impacts, but that these impacts would be reduced to levels that are not significant with the adoption of mitigation measures and the implementation of a mitigation monitoring plan.

CONDITIONS OF APPROVAL for Minor Subdivision Map: The following Conditions of Approval shall be made a permanent part of Minor Subdivision Map No. 06-18, shall remain in force regardless of property ownership, and shall be implemented in order for this entitlement to remain valid:

1. The Tentative Map shall expire twenty-four (24) months from the date of its approval or conditional approval unless extended in accordance with the City of Ukiah Subdivision Ordinance and the Subdivision Map Act.
2. All taxes now due, or past due, must be paid prior to the approval of the Parcel Map.
3. Each parcel shall be served individually upon the development of the parcel with appropriate public utilities required for the type of development within the parcel.
4. Easements will be required for all private site access roads, driveways, and parking areas used in common, and for any drainage that affects abutting properties. These easements shall be included on the Parcel Map and recorded with the Mendocino County Clerk/Recorder prior to the recordation of the Parcel Map.
5. The Parcel Map, which shall be prepared pursuant to Local Ordinance and the Subdivision Map Act, easements, title report and closures shall be submitted to the Public Works/City Engineer Department, with a deposit of \$65, for review and approval by the City Engineer. The entire balance of Parcel Map processing fees shall be paid prior to the recordation of the Parcel Map.
6. The existing Rite-Aid drugstore structure that is located on Proposed Parcel Two of this subdivision shall be demolished prior to the recordation of the Parcel Map.
7. In the event that a building permit is requested for construction of a residential structure on either of the parcels established by this subdivision map within four years of the recordation of the map, the owner of the parcel shall submit a park dedication fee calculated in accordance with standards and formulas in effect at the time such building permit is requested.

FINDINGS for Major Site Development Permit No. 06-19: The Planning Department recommendation for the approval of the project's development component is based, in part, on the following findings:

1. The development of the drugstore, as conditioned, is consistent with the Ukiah General Plan, including the goals and policies and siting criteria of the C (Commercial) land use classification and the "gateway" design criteria of the Community Design Element;
2. The development of the drugstore, as conditioned, is consistent with the applicable use and development standards of the C-1 (Community Commercial) zoning district;
3. The development of the drugstore, as conditioned, is consistent with the applicable design guidelines of the Downtown Design Guidelines for Commercial Development in the City of Ukiah;
4. The development of the drugstore, as conditioned, is consistent with the compatibility criteria for the C Compatibility Zone, as outlined in the Ukiah Municipal Airport Master Plan and the County of Mendocino Airport Land Use Plan;
5. The development of the drugstore, as conditioned, would not create hazardous or inconvenient impacts to existing vehicular or pedestrian patterns since there will be no appreciable increase in levels of average daily traffic from the site, resulting in no decline in the service levels for abutting public streets or the abutting intersections and will provide effective and distinctive pedestrian access between the abutting public sidewalks and the entrance of the building;

6. The City Engineer determined the circulation pattern and access driveway for the drugstore development, is consistent with the applicable roadway improvement standards and would provide safe and efficient ingress and egress;
7. The development of the drugstore, as conditioned, would have sufficient landscape areas and landscape plantings to provide screening of the buildings from Gobbi Street and State Street and abutting properties and to break up and screen on-site parking areas;
8. The development of the drugstore, as conditioned, would not restrict or cut out light or air on abutting parcels since it is centrally located on the site and will be at least 25 feet from the properties to the north, resulting in adequate setbacks from the buildings and building sites on the parcel to the north to avoid screening or overshadowing of buildings;
9. The development of the drugstore, as conditioned, would not excessively damage or destroy natural features since the only such features consist of ornamental trees that will be replaced with trees that are considered to be more aesthetically compatible with the site design and building designs for the project;
10. The drugstore development, as conditioned, contains sufficient variety in building design, building materials, and site design to provide sufficient variety, creativity, and articulation that will avoid monotony and/or a box-like or uninteresting external appearance, including the use of varied wall offsets, variegated parapet roof designs, trellises, contrasting but compatible masonry building materials, and a creative landscape plan;
11. A Mitigated Negative Declaration prepared for the Minor Subdivision Map (MS 06-18) and a Site Development Permit (SDP 06-18) to allow the subsequent development of one of the lots established by the subdivision determined that the project would cause potentially significant adverse environmental impacts, but that these impacts would be reduced to levels that are not significant with the adoption of mitigation measures and the implementation of a mitigation monitoring plan; and
12. The development of the drugstore, as conditioned, would not be detrimental to the public's health, safety and general welfare since it is consistent with applicable General Plan goals and policies, the use and development standards of the Ukiah Municipal Code, the compatibility criteria for the Ukiah Municipal Airport, and the design guidelines for the Downtown Design District.

CONDITIONS OF APPROVAL: The following Conditions of Approval shall be made a permanent part of Major Site Development Permit 06-19, shall remain in force regardless of property ownership, and shall be implemented in order for this entitlement to remain valid:

1. All use, construction, or occupancy shall conform to the application approved by the Planning Commission, and to any supporting documents submitted therewith, including maps, sketches, renderings, building elevations, landscape plans, and alike.
2. Any construction shall comply with the "Standard Specifications" for such type of construction now existing or which may hereafter be promulgated by the Engineering Department of the City of Ukiah; except where higher standards are imposed by law, rule, or regulation or by action of the Planning Commission.
3. In addition to any particular condition, which might be imposed, any construction shall comply with all building, fire, electric, plumbing, occupancy, and structural laws, regulations and ordinances in effect at the time the Building Permit is approved and issued.
4. Applicant shall be required to obtain any permit or approval, which is required by law, regulation, or ordinance, be it required by Local, State, or Federal agency.

5. Building Permits shall be issued within two years after the effective date of the Site Development Permit, or the discretionary actions granted by the permit shall expire. In the event the Building Permit cannot be issued within the stipulated period from the project approval date, a one year extension may be granted by the Director of Planning if no new circumstances affect the project which otherwise would render the original approval inappropriate or illegal. It is the applicant's responsibility in such cases to propose the one-year extension to the Planning Department prior to the two-year expiration date.
6. The approved Site Development Permit may be revoked through the City's revocation process if the approved project related to the Use Permit is not being conducted in compliance with the stipulations and conditions of approval; or if the project is not established within two years of the effective date of approval; or if the established land use for which the permit was granted has ceased or has been suspended for twenty four (24) consecutive months.
7. Except as otherwise specifically noted, the Site Development Permit shall be granted only for the specific purposes stated in the action approving the Use Permit and shall not be construed as eliminating or modifying any building, use, or zone requirements except to such specific purposes.
8. The existing retail store building on the west side of the site shall be closed for any operations once an Occupancy Certificate for the new retail store building on the east side of the site is secured and the building shall be demolished within one year of the issuance of the Occupancy Permit or prior to the recordation of the Parcel Map, whichever occurs first.
9. A schedule providing detailed information regarding the demolition of the existing drugstore building, the disposal or recycling of demolition materials, and the methods and timing for covering all exposed areas left by the demolition shall be submitted to the Ukiah Department of Planning and Community Development for review and this shall be approved by its staff prior to the issuance of any demolition permits for the building or for any site preparation or construction on either of the proposed parcels. (MM)
10. The number and size of freestanding and building signs shall be restricted to those prescribed by the Sign Code provisions of the Ukiah Municipal Code or permitted in a final sign program approved by the Ukiah Planning Commission as part of the discretionary permit approval for the project. (MM)
11. Prior to the issuance of any Building Permit for the construction of the retail store building, a Final Lighting Plan shall be submitted to the Director of Planning and Community Development or his/her designee for review for compliance with Ukiah Municipal Code standards for on-site lighting and with "dark sky" guidelines for reducing nighttime lighting on the site. The Final Lighting Plan shall include details regarding exterior lighting for structures, garden areas, and walkways, with lighting sources that are full cut-off, hooded, and down-cast, or otherwise shielded to ensure that light does not adversely shine towards neighboring properties, or toward the night sky. Additionally, all lighting shall be the minimum wattage necessary to provide adequate security, yet shall not result in excessively bright night glow. Sufficient details regarding the proposed wattage of all site lights shall be included in the Final Lighting Plan so that the Planning Staff can determine how bright the proposed site lights will be. The Director of Planning and Community Development shall have the authority to require the Final Lighting Plan to be modified (including the wattage) and/or additional information to be submitted so that the lighting meets the requirements listed above. (MM)
12. A valid air quality permit issued by the Mendocino County Air Quality Management District for any cumulative grading disturbances of one acre or more shall be obtained prior to the commencement of such grading activities.

13. Construction activities shall be conducted with adequate dust suppression methods, including watering during grading and construction activities to limit the generation of fugitive dust or other methods approved by the Mendocino County Air Quality Management District. Prior to initiating soil removing activities for construction purposes, the applicant shall pre-wet affected areas with at least 0.5 gallons of water per square yard of ground area to control dust. (MM)
13. The burning of construction debris is prohibited. Any disposal of vegetation removed as a result of lot clearing shall be lawfully disposed of, preferably by chipping and composting, or as authorized by the Mendocino County Air Quality Management District. (MM)
14. During construction activities, the applicant/owner/contractor shall remove daily accumulation of mud and dirt on paved access lanes that serve the project site. (MM)
15. The applicant/owner shall secure a grading permit from the Planning and Community Development Department and shall adhere to all grading permit conditions, including Best Management Practices. All areas disturbed by grading shall be either paved with concrete or asphaltic concrete, landscaping or erosion control hydroseed mixture. (MM)
16. All refuse generated by the facility shall be stored in approved disposal/storage containers, and appropriately covered. Removal of waste shall be on a weekly basis at a minimum so as to avoid excess waste. All trash receptacles/containers shall remain covered at all times to prevent fugitive odors and rodent infestation. (MM)
17. Demolition of the existing Rite Aid structure will require asbestos clearance and notification to the Mendocino County Air Quality Management District. The City of Ukiah Building Division cannot issue a demolition permit until they have received clearance from the MCAQMD. (MM)
18. Any stationary on-site internal combustion engines over 50 horsepower (i.e. generators) may require a permit from the MCAQMD depending upon fuel source and level of operation. (MM)
19. The use of the pharmacy drive-thru feature shall be limited to pharmacy customers only (i.e. no general retail customers allowed). Prior to use of the drive-thru feature, the applicant must demonstrate to the MCAQMD that vehicle emissions associated with the drive-thru feature will not contribute significantly to air quality in this area or the valley in general. (MM)
20. All grading activities must comply with MCAQMD Regulation 1 Rule 430 regarding fugitive dust emissions. (MM)
21. In the event that archaeological remains or artifacts are uncovered during construction activities, work shall be stopped and a qualified archaeologist shall survey the site with all costs borne by the applicant/owner. The archaeologist shall submit a report to the City Planning and Community Development Department with recommendations on the disposition of the site. Disposition may include, but is not limited to, excavation and documentation, capping the site, or leaving the site in an open space area. The recommendations of the archaeologist shall be incorporated in the project as project mitigation. (MM)
22. The proposed development shall be designed and constructed in accordance with the most current applicable Building Codes, including the Uniform Building Code (UBC) and the California Building Code (CBC) as determined by the Building Division of the Planning and Community Development Department of the City of Ukiah. (MM)

23. Should the proposed use of the site require the storage of materials equal or greater than 55 gallons of a liquid, 500 pounds of a solid or 200 cubic feet of compressed gas, the applicant will be required to submit a Hazardous Materials Business Plan to the Planning and Community Development Department with said plan to be renewed and updated annually. If quantities are less than the amounts noted above, the applicant must submit a Hazardous Materials/Waste registration form to the Department. (MM)
24. Equipment fueling and service shall be conducted at a designated location other than the project site, including local gas stations or repair shops. (MM)
25. Any spills resulting from fueling or hydraulic line breaks will be contained and cleaned up immediately. (MM)
26. During construction activities, equipment shall be maintained and kept in good operating conditions to reduce the likelihood of line breaks and leakage. (MM)
27. No refueling or servicing shall be done without absorbent materials (i.e. absorbent pads, mats, socks, pillows, and granules) or drip pans underneath to contain spilled materials. (MM)
28. Prior to the issuance of building permits, the preparation of an on-site Grading and Drainage Plan, an Erosion and Sediment Transport Control Plan, and drainage calculations showing that the proposed drainage facilities are adequate shall be prepared by a Registered Civil Engineer for approval by the City Engineer. No drainage is allowed to be routed over sidewalks. Storm drain inlets must be protected by a fossil fuel filtering device if required by the City Engineer. (MM)
29. Prior to the commencement of any grading activity on-site, the applicant/contractor shall install structural control measures to reduce erosion and retain sedimentation. Measures may include, but are not limited to, stabilization of control entrance, temporary gravel and construction entrance, installation of a temporary gravel construction entrance, and installation of filter fabric fences. The applicant shall remove all temporary erosion control facilities upon stabilization of the entire project site, as approved by the City Engineer. (MM)
30. During construction, the applicant and the contractor of record shall exercise BMPs, such as daily street sweeping and the placement of erosion control measures on-site, to minimize storm water pollution. The contractor shall designate a primary contact person who shall be available to the City of Ukiah in the event of noted storm damage or storm event. Said person shall be responsible for inspection of all erosion control facilities. (MM)
31. The hours of construction shall be limited to 7:00 A.M to 6:00 P.M. Monday through Friday, and 8:00 A.M. to 4:00 P.M. on Saturday, with no construction to occur on Sundays and holidays. (MM)
32. Maximum level of noise that can emanate from long term commercial noise sources is limited to 60 decibels during the hours of 10:00 p.m. to 7:00 a.m. and 65 decibels from 7:00 a.m. to 10:00 p.m. (MM)
33. The City of Ukiah Fire Department shall review and approve a full set of construction plans prior to the issuance of building permits by the Building Division of the City of Ukiah Planning and Community Development Department. These plans shall include a sprinkler plan and an alarm plan. The site plan shall be updated to illustrate the location of the required fire hydrants. Plan checking and inspection fees shall be the responsibility of the applicant and shall be collected prior to issuance of permits. (MM)
34. The automatic extinguishing system (sprinklers) and the fire alarm system shall meet the standards of the Uniform Fire and Building Codes as approved by the City of Ukiah Fire Department. (MM)
35. Street numbers, door and exit signs, fire extinguishers, fire lanes and no parking areas, electrical shut-off access, secure lock box, fire alarms, smoke detectors and other standard requirements shall be annotated on required construction plans. (MM)

36. Prior to the commencement of construction activities, the applicants shall submit a Storm Water Pollution Prevention Plan to the Regional Water Quality Control Board (RWQCH) and secure a General Construction Activity Permit if required by law.
37. The applicants shall pay the required sewer/water connection fees at the time of application for service.
38. Sewage collectors for the project shall be sized and constructed according to the requirements of the Ukiah Valley Sanitation District.
39. Prior to the issuance of a building permit, the applicants shall pay the required Ukiah School district fees applicable to commercial development.
40. Prior to the final inspection of the building, and issuance of a certificate of occupancy, the applicants shall submit a Recycling Plan for all recyclable materials, if any, the drugstore will generate during its operation. This plan shall comply with all requirements of the Ukiah Municipal Code.
41. Prior to the issuance of an Occupancy Permit any existing curb and gutter adjacent to the property that is in disrepair shall be replaced per City Standard Drawings 101 and 102 or with other requirements specified by the City Engineer.
42. Existing sections of driveways that will be abandoned as part of this project shall be removed and replaced with standard curb and gutter.
43. New sidewalks along the Gobbi Street frontage and an ADA ramp at the corner of Gobbi Street and State Street shall be constructed in accordance with the alignment and widths determined by the City Engineer and applicable City Standard Drawings. Improvement plans for the sidewalk and ramp shall be prepared by a licensed Civil Engineer and submitted to the City Engineer/Public Works Director for review and shall be approved prior to the issuance of a site improvement permit or other ministerial permit to construct the sidewalk or ramp.
44. Street trees within five feet (5') of the curb or in landscape planters abutting the sidewalk shall be incorporated into the Final Landscape Plan for planting approximately every 30 feet along the street frontages for State Street, Gobbi Street, and South Oak Street. Tree types and the locations of the trees shall be reviewed and approved by the City Engineer prior to the issuance of a Building Permit and planted in accordance with City Standard Drawings or any other standard in effect at the time the trees are planted.
45. An Encroachment Permit shall be obtained from the City by a licensed contractor prior to doing any construction within the public street easements. Encroachment permit fee shall be \$45 dollars plus three percent (3%) of estimated construction costs. Contractor(s) shall have a current City of Ukiah Business License who shall submit copies of proper insurance coverage (Public Liability: \$1,000,000; Property Damage: \$1,000,000) and current Workman's Compensation Certificate.
46. All on-site paving shall be a minimum of 2" (inches) of asphalt concrete with a 6" (inch) aggregate base, or, alternatively, any option approved by the City Engineer. If any portion of the paved surface is to be used regularly by heavy vehicles (such as solid waste pick-up vehicles), the pavement section should be constructed to withstand anticipated loads.
47. Sewer, water, and electric service shall conform to the specifications of the City Public Utilities and Public Works Departments.

48. A Final Parking Plan shall be submitted by the project applicant and shall be approved by the Director of Planning and Community Development prior to the issuance of a Building Permit and all parking facilities and amenities included in this plan shall be installed on the site prior to the issuance of a Certificate of Occupancy for the building. This plan shall include, but not be limited to the following:
 - a. The designation of 5-7 employee parking stalls in the row of parking stalls on the westerly side of the drugstore site; and
 - b. The provision of bicycle parking for a minimum of 6 customers in a paved area near the entrance of the building and for 3 employees within the building or in a secure area outside the building, with weather coverings where feasible.
49. A Landscaping and Lighting Plan shall be submitted by the project applicant and shall be approved by the Director of Planning and Community Development prior to the issuance of a Building Permit and all plants shall be planted in accordance with the approved landscape plan prior to the issuance of a Certificate of Occupancy for the building. This plan shall include, but not be limited to, the following information and criteria:
 - a. A planting legend that includes the names, location, coverage area, and canopy cover of proposed vegetation;
 - b. A planting schedule for all vegetation installed on the site;
 - c. A maintenance and watering schedule for all vegetation;
 - d. The inclusion of required street trees within abutting sidewalks along State Street, Gobbi Street, and South Oak Street, or in the setbacks abutting these streets if approved by the City Engineer;
 - e. The inclusion of trees that are more proportional to the height, mass, and scale of the building, including deciduous and evergreen tree species; and
 - f. The submittal of a Shading Plan prepared by a landscape professional that shows the areas of the parking lot that will be shaded by the proposed shade trees when mature and the inclusion of any additional trees required to bring the shaded area to the required coverage of 50 percent of paved areas;
 - g. The submittal of a report on the health and feasibility of retaining the existing trees located in the planter area abutting State Street; and
 - h. The design of pedestrian walkways between the public street frontages for State Street and Gobbi Street and the entrance to the building.
50. Landscaping shall be maintained in a neat, weed-free manner, and may not be removed or substantially altered unless the Director of Planning reviews and approves the removal or replacement of vegetation determined to be diseased, unstable, hazardous, or poorly located on the site. Any vegetation removed from the interior portions of the site shall be replaced with similar vegetation approved by the Planning Director and any street trees removed shall be replaced with vegetation approved by the City Engineer.
51. Any roof-mounted air conditioning, heating, and/or ventilation equipment shall be aesthetically screened from view.

52. Outdoor refuse containers, recyclable containers, and other outdoor storage shall be aesthetically screened from view by solid fences using building materials that are compatible with those utilized on the primary structure or with vegetative barriers that are consistent with the landscaping scheme approved by the Director of Planning. Under no circumstances shall garbage, recyclables, or materials being store outdoors be visible outside the enclosures.
53. All conditions of approval shall be listed on the title page of any plans submitted for development activities on the site and it shall be the responsibility of the applicant/developer to ensure that all contractors responsible for any work on the project fully understand these conditions and comply fully with their implementation. Any deviation from the terms of these conditions or the plans/exhibits approved as part of the project shall be submitted to the Director of the Planning and Community Development for review and shall be approved prior to any work affected by the modification.
54. All conditions that do not contain a specific date or time period for completion shall be completed prior to the issuance of a Certificate of Occupancy.
55. Requests for the final inspections required for the approval of an Occupancy Permit for the drugstore building shall be submitted by the applicant/developer or his/her contractor to the Department of Planning and Community Development's Building Division no later than one week prior to the time requested for inspections. The applicant/developer or his/her contractor is encouraged to contact staff at any time to discuss the timing for the completion of required conditions or any other matter that could cause delays in obtaining an Occupancy Permit.

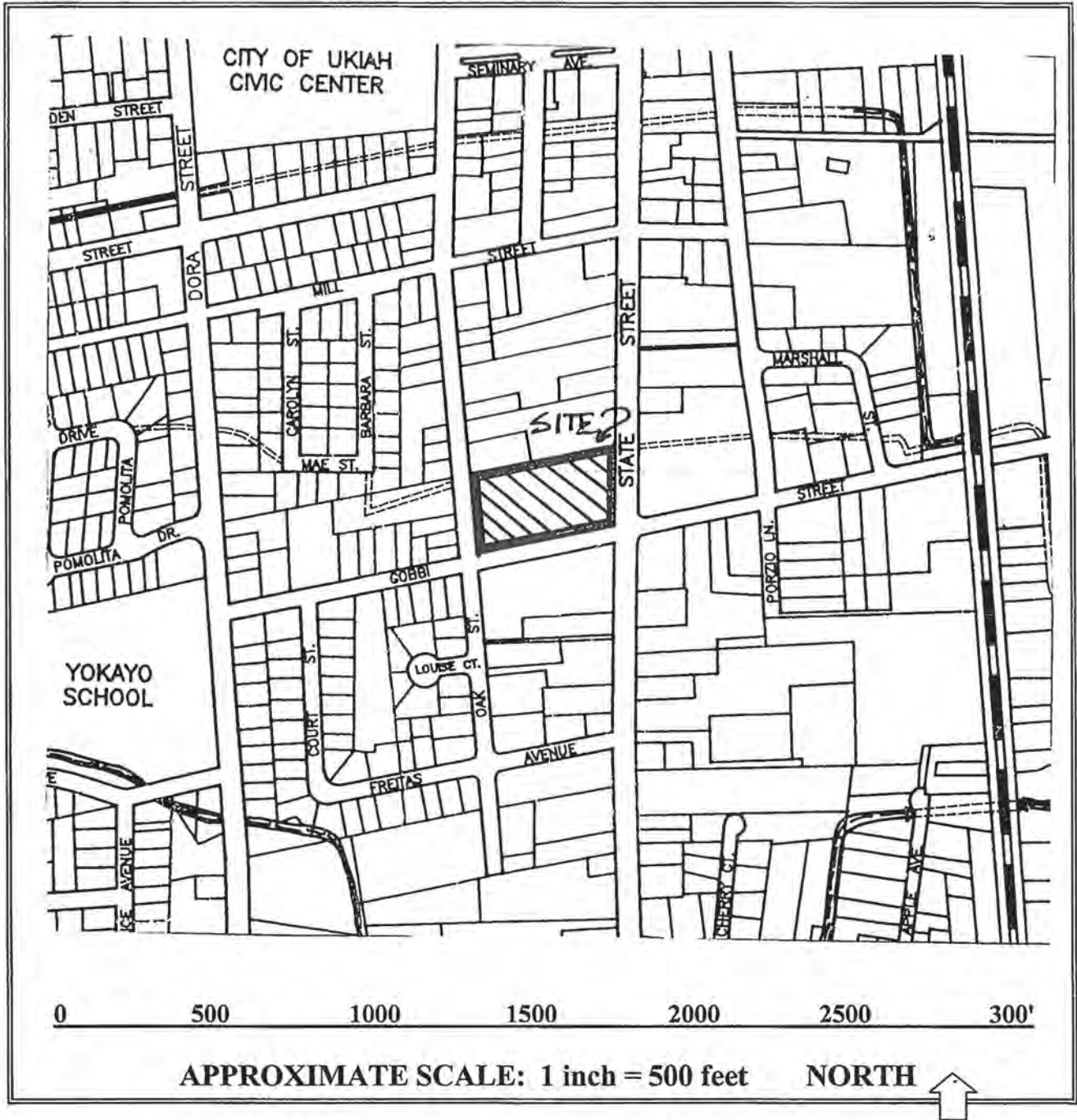
ATTACHMENTS

1. Location Map
2. Project Narrative prepared by Applicants
3. Site Plan for Existing Rite-Aid Store (reduced; not to scale)
4. Site Plan for Proposed Rite-Aid Store/Subdivision (reduced; not to scale)
5. Building Elevations (reduced; not to scale)
6. Conceptual Sign Program (reduced; not to scale)
7. Mitigated Negative Declaration with Mitigation Measures & Mitigation Monitoring Report

STAFF REPORT PREPARED BY: Dave Lohse, Associate Planner

LOCATION MAP

Rite-Aid Drugstore Minor Subdivision and Major Site Development Permit
MS 06-18 & SDP 06-19
680 South State Street





DEC 01 2006

CITY OF UKIAH
PLANNING DEPT

December 1, 2006

Rite Aid Site Redevelopment / 680 South State Street, Ukiah

Rite Aid Corporation proposes to redevelop the existing Rite Aid site located at 680 South State Street in Ukiah, California. The existing zoning is C-1, which appropriate for the current and proposed retail use, so no change to the zoning or General Plan designation is anticipated. A minor subdivision of the property and a site development permit for the front portion of the project site are being submitted at this time, and appropriate permits will be sought for redevelopment of the rear portion of the property at a later time.

Minor Subdivision:

Rite Aid proposes to create two (2) lots via the Minor Subdivision process, and construct a new retail store, with associated parking, on the new lot closest to the intersection of South State and West Gobbi Streets (Parcel One, Sheet A0.2). The other lot (Parcel Two on Sheet A0.2) encompasses the area of the existing Rite Aid building footprint plus the travel aisle and parking adjacent to the south of that building (abutting West Gobbi and Oak Streets). Parcel One as proposed will be 68,488 square feet in size, while Parcel Two will be 38,001 square feet in size.

As a part of the subdivision process, City staff has indicated that dedication of additional right-of-way along West Gobbi Street will be required, in order to accommodate a bike lane for the length of that block and to improve vehicular circulation at the intersection. Utilities and traffic-control mechanisms at the northwest corner of the intersection will be relocated as needed to widen the turn-radius for vehicles turning right onto West Gobbi from South State Street, using a new right-of-way acquired by the City of Ukiah. The existing curb cut on Gobbi Street immediately west of South State Street will be closed, and a wide, landscape-lined pedestrian access to the new retail space will be created to access the new building, which will be situated much closer to the intersection than the existing building.

Site Development Permit:

The new retail building will be approximately 17,422 square feet, with 61 parking spaces, of which three (3) will be accessible. Said parking is located on three sides of the building, eliminating the one large parking area that currently exists at the front of the existing store, and spreading parking more evenly throughout the site. No parking will be allowed on the north side of the new building, which will accommodate a drive-thru pharmacy facility. Landscaping, which as proposed comprises approximately 15% of the site area of Parcel One, will consist of tree islands throughout the parking areas and landscaped areas along the perimeter of both the South State and West Gobbi Street frontages, and at the rear of

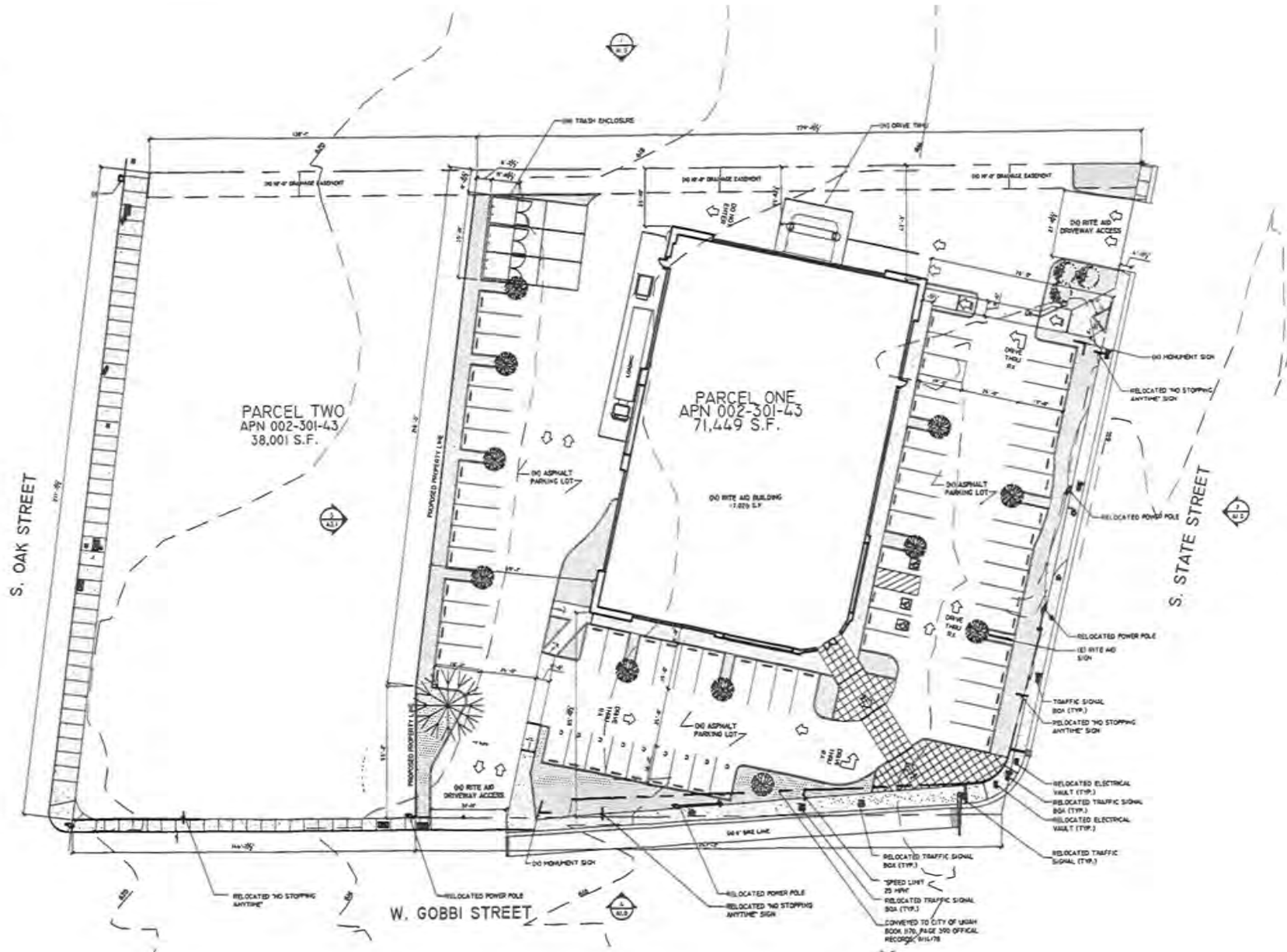


Parcel One along the common property line of Parcels One and Two. A detailed landscape plan has been developed for inclusion in the information provided to the Planning Commission.

The building's interior layout is a standard corporate design, developed by Rite Aid and one of several floor plans used nation-wide. The new Rite Aid will feature a two-lane drive-thru pharmacy facility on the north end of the building, from which cars will exit by way of the West Gobbi Street curb cut located at the southwest corner of Parcel One.

The exterior design has been modified by Ruff + Associates, Inc. to incorporate some of the historic design elements thought to be characteristic of Ukiah. Building façade materials will be stucco and brick with stone trim. A cantilevered metal trellis is proposed on a portion of the east and south sides of the building to provide shade over the sidewalk. A color rendering and materials board has been provided to show proposed colors and materials selections.

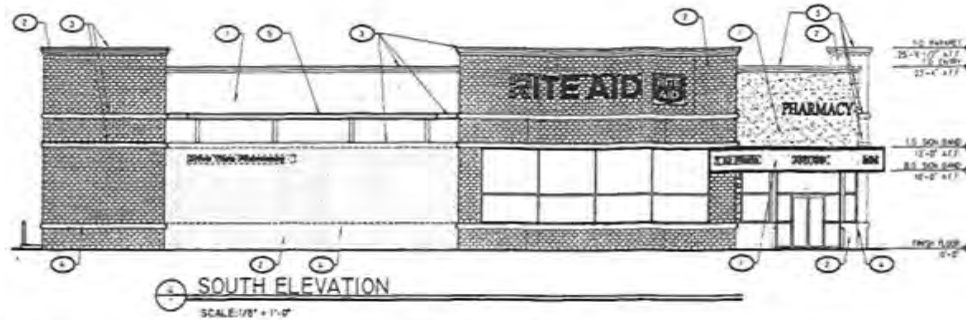
Additional information on the monument sign, designed by Paramount Signs, has been included in this submittal package. We are still awaiting emissions information, to be provided by Rite Aid, and will forward that info to City staff immediately upon receipt.



DATE	
NO. 0004	
RUFF + ASSOCIATES Architecture Planning Development	
100 West Sanitary Street, Ukiah, CA 95482 Phone: 707-472-0525 Fax: 707-472-0527 e-mail: ruff@ruffassoc.com	
COPYRIGHT © 2007 BY RUFF + ASSOCIATES ALL RIGHTS RESERVED.	
RITE AID CORPORATION NEW BUILDING 680 SOUTH STATE ST., UKIAH, CA 95482	
DATE	
SITE PLAN	
DESIGNED BY	BJM
CHECKED BY	BJM

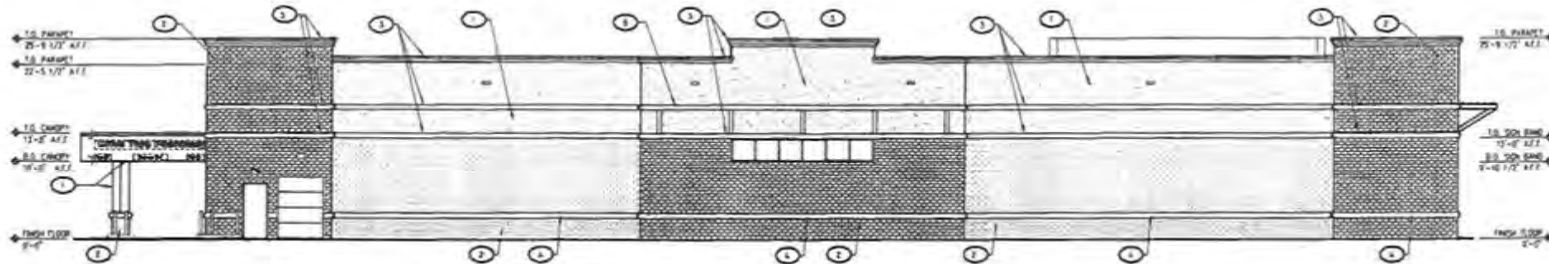
KEY NOTES (1)

- 1 STUCCO FINISH
- 2 BRICK VENEER
- 3 STUCCO COVERED FORM FIN.
- 4 STONE HOLDING
- 5 METAL TRUSS



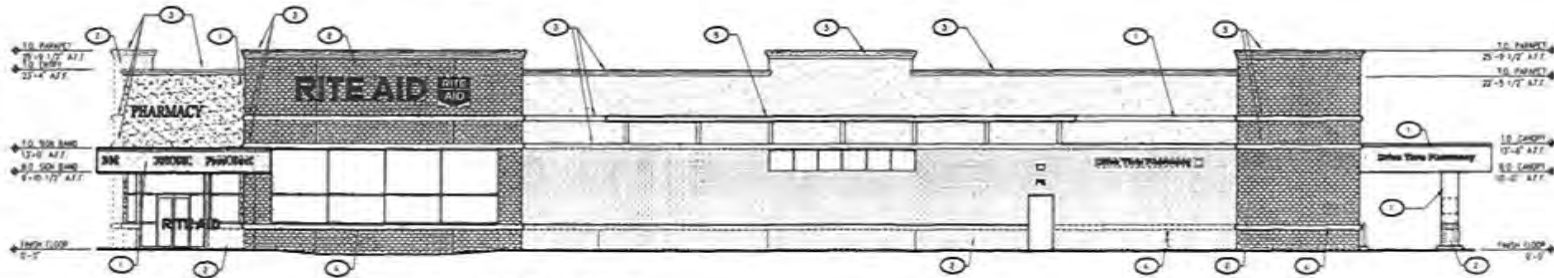
SOUTH ELEVATION

SCALE: 1/8" = 1'-0"



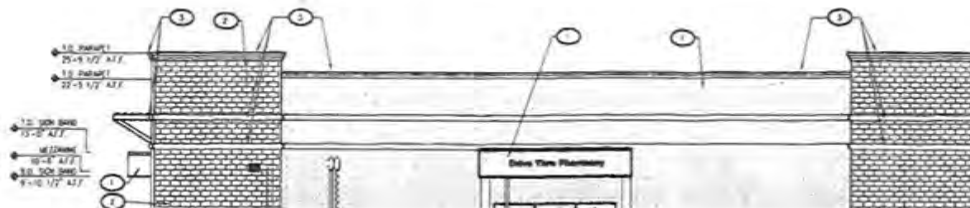
WEST ELEVATION

SCALE: 1/8" = 1'-0"



EAST ELEVATION

SCALE: 1/8" = 1'-0"



DATE	
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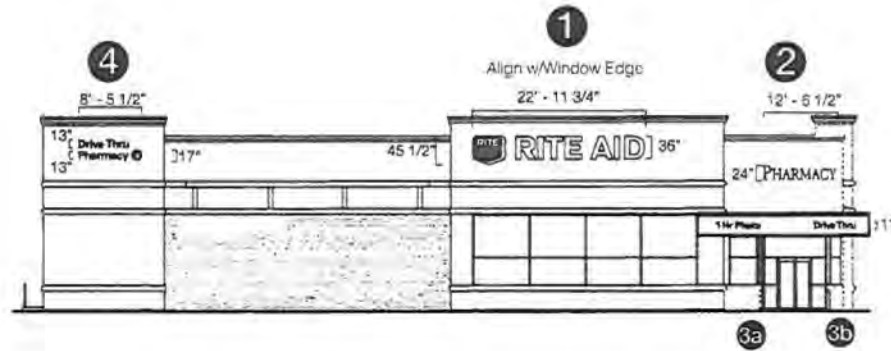


RUFF + ASSOCIATES
 Architecture, Planning, Development
 100 West Sandhill Street, Ukiah, CA, 95482
 Phone: 707-472-0123 Fax: 707-472-0127
 e-mail: ruff@ruff-associates.com

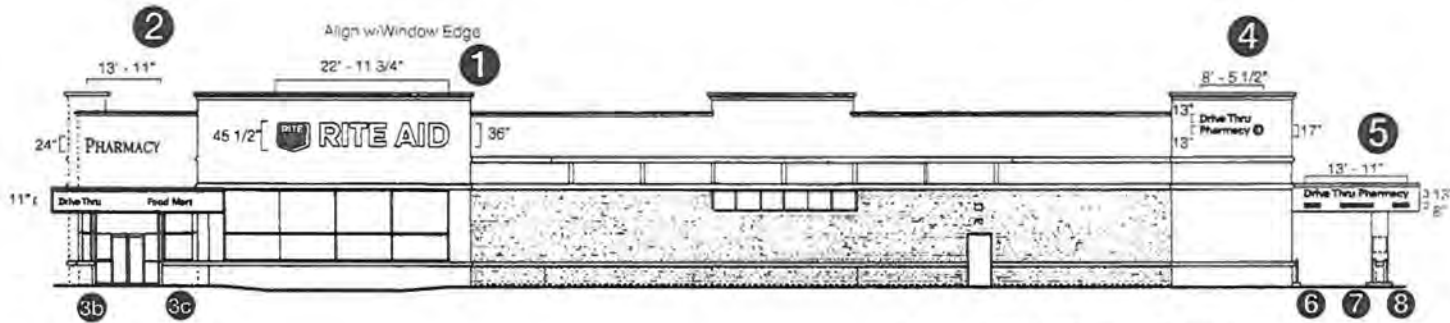


RITE AID CORPORATION
 NEW BUILDING
 680 SOUTH STATE ST., UKIAH, CA 95482

DATE: 01/11/01
 SHEET NO.: 5
 SITE PLAN



Short Side Front Elevation



Long Side Front Elevation

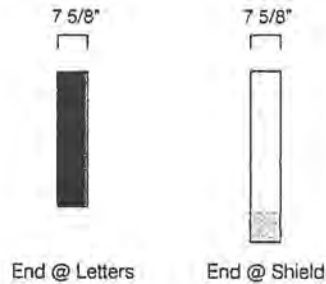
Scale: 1/16" = 1'

Building Elevations

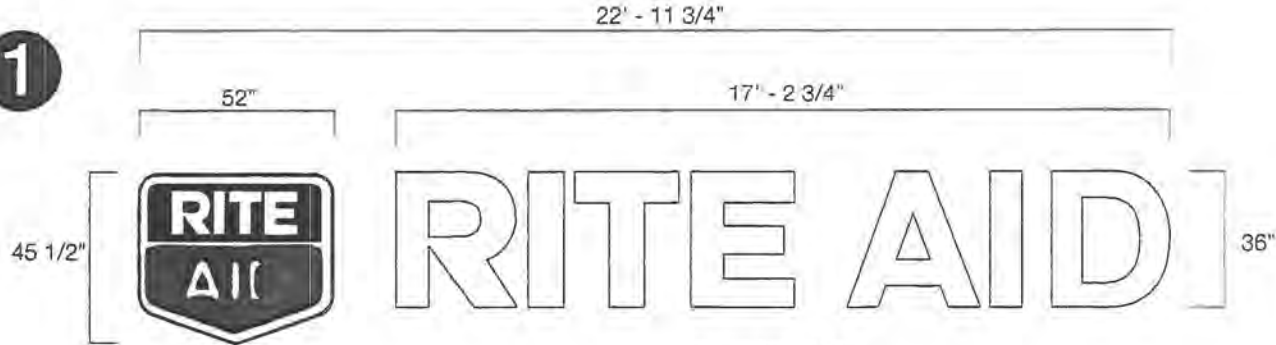
Account Rite Aid 6033 Drawn By J.Mullen
 Project Title 680 South State St. Ukiah, CA IP Rep. Ochoa
 Approved By
 Date Created 29, August, 06 Date Revised 31, August, 06

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1



Scale: 3/8 = 1'-0"

Square Footage

Letters	3'0" x 17'-2 3/4" = 51.69 Sq.Ft.
Shield	3'9 1/2" x 4'-4" = 16.43 Sq.Ft.
Total	= 68.12 Sq.Ft.

Shield Logo

Face
White SG-410 .150 Lexan w/vinyl graphics.

'Rite' Copy
White in Blue vinyl field Calon 2870.

'Aid' Copy
White in Red vinyl field Calon 2283.

Returns
Fab. pre-coat semi-gloss White .040 alum. wrap. Fab. alum. retainers 5/8" wide. Shield back is .090 alum.

Illumination
Daylight HO fluorescent lamps.

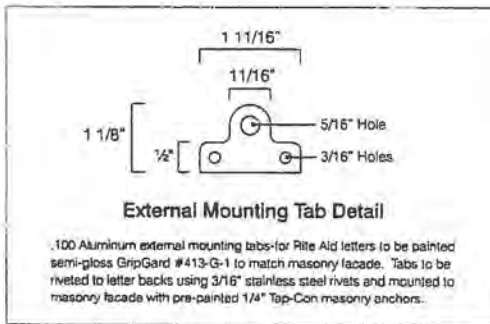
Rite Aid Letters

(2) Two Sets Required

Faces
White SG-410 .150 Lexan.

Returns
Fab. .040 alum. wrap, paint GripGuard Blue TM Calon 2870, semi gloss finish. Fab. alum. retainers 5/8" wide, Blue. Letter backs are .090 alum. Note: Paint inside letters AKZO Bright White reflective finish.

Illumination
(3) Rows, 15mm 6500 White EGL neon. 80ma., self contained 120V neon transformers.



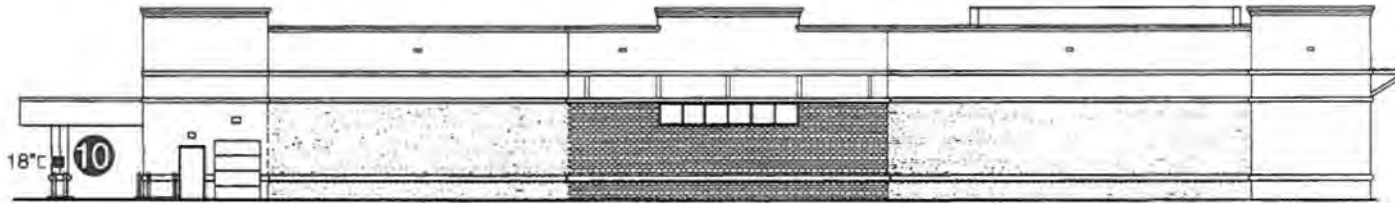
PN:

36" Channel Ltrs w/45" Shield

Account Rite Aid 6039
 Project Title 680 South State St, Ukiah, CA
 Date Created 29, August, 06
 Drawn By J.Mullen
 IP Rep. Ochoa
 Approved By
 Date Revised

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Long Side Rear Elevation



Short Side Rear Elevation

Scale: 1/16" = 1'

Building Elevations

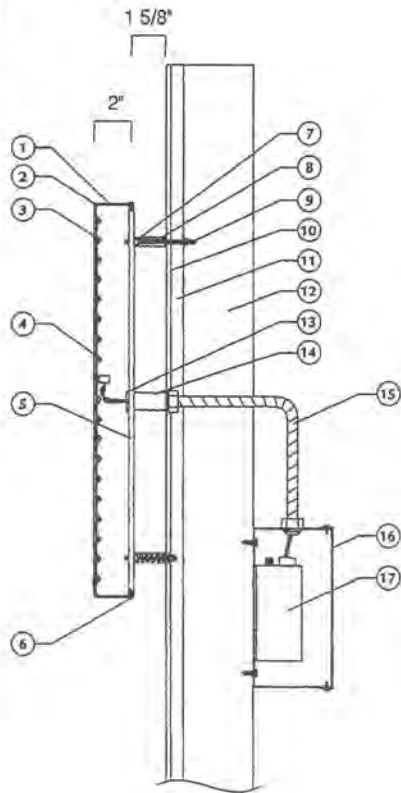
Account Rita Ald 6033
Project Title 680 South State St
Ukiah, CA
Date Created 29, August, 06

Drawn By J.Mullen
IP Rep. Ochoa
Approved By
Date Revised

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06-0829 Pg 2 of 13

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- ① .063 Aluminum letter returns.
- ② Spot welds as needed.
- ③ .125 Aluminum letter faces.
- ④ Sloan white LEDs
- ⑤ Clear Lexan back w/diffuser applied to interior surface.
- ⑥ #6 x 1/4" blue s.m. stainless steel fasteners.
- ⑦ #10 - 24 Aluminum insert nut.
- ⑧ 3/8" x 18 Gauge aluminum tube spacer.
- ⑨ Anchors as required.
- ⑩ Facade by others.
- ⑪ Plywood sheathing by others.
- ⑫ Vertical steel stud every 16" O.C. by others.
- ⑬ 1/2" Chase nipple.
- ⑭ 1/2" Aluminum electrical coupling
- ⑮ 3/8" Liquidtight flexible conduit w/"twist-in" connectors
- ⑯ 4" x 8" x 18" JIC box.
- ⑰ Low voltage LED power supplies (120 volt circuit req'd)

Note: Building dimensions and exact construction of decor band to be verified by sign installer prior to installation of signs. All penetrations to be sealed with BIOSTOP 500+ Intumescent Firestop Sealant to meet NEC #300.21, ASTM #E814, UL #1479 and UL #2079 standards.

2

24" End



Scale: 3/8 = 1'-0"

Sign Area: 25 sf

Faces

.125 Alum., paint GripGuard Blue TM Calon 2870, semi gloss finish.

Returns

Fab. .063 alum., 2" deep, paint GripGuard Blue TM Calon 2870, semi gloss finish.

Notes: Backs are Clear Lexan w/White vinyl diffuser 3M 3635-70. Paint inside letters AKZO Bright White reflective finish.

Illumination (Halo)

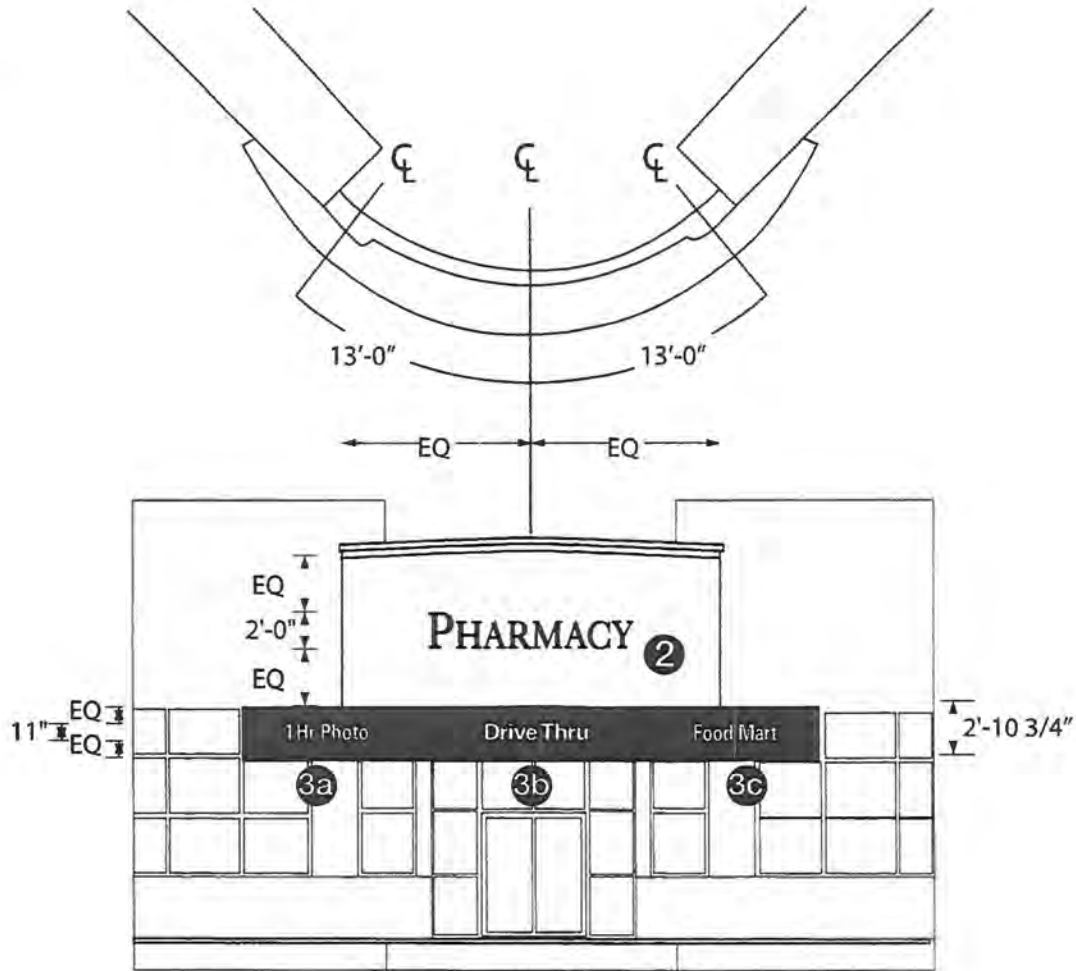
Sloan White LEDs. Note: Peg letters off wall 1 5/8" for proper illumination.

PN:

24" Reverse Pan Channel Ltrs/Halo Lit

Account: Rite Aid 6033
 Project Title: 680 South State St. Ukiah, CA
 Date Created: 29, August, 06
 Drawn By: J.Mullen
 IP Rep.: Ochoa
 Approved By:
 Date Revised:

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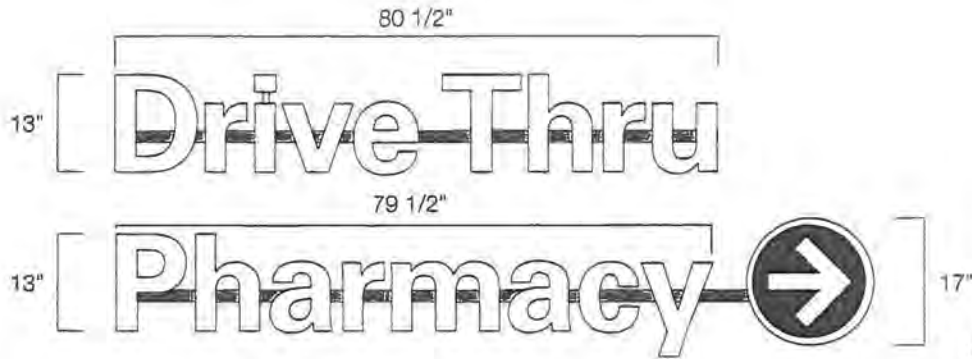
Corner Elevation

Account: **Rile Ald 6033**
 Project Title: **680 South State St Ukiah, CA**
 Date Created: **29, August, 06**
 Drawn By: **J. Mullen**
 IP Rep: **Ochoa**
 Approved By:
 Date Revised:

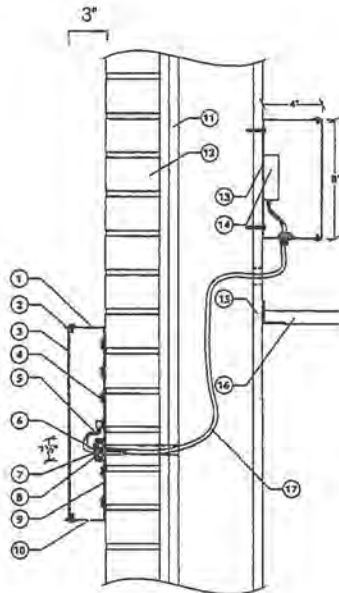
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Sign Area: 17 sf

4



Scale: 3/4" = 1'-0"



- ① .040 Aluminum returns, 2 1/2" deep.
- ② 3/4" Jewelite trim cap.
- ③ White Lexan face.
- ④ Sloan white LEDs.
- ⑤ Low voltage wiring from power supply.
- ⑥ Pre-drilled electrical hole w/plastic grommet.
- ⑦ Aluminum raceway.
- ⑧ Masonry fasteners (min. 3 per raceway).
- ⑨ .090 Alum. letter back. Sides and backs stitched together and siliconed.
- ⑩ 1/4" Weep holes w/internal light baffle.
- ⑪ Sheathing by others.
- ⑫ Building facade.
- ⑬ 4" x 8" x 18" JIC box.
- ⑭ Low voltage LED power supplies (120 volt circuit req'd).
- ⑮ Interior Sheathing by others.
- ⑯ Interior drop ceiling.
- ⑰ High voltage rated flexible wire sleeve.

Illumination
Sloan White LEDs.

Wireways
Fabricated 1 1/2" x 1/2" deep alum., painted GripGard #415-C-2 tan to match SW #2195 Roadside.
Note: Access to rear of facade to by others.

Note: Building dimensions and exact construction of decor band to be verified by sign installer prior to installation of signs. All penetrations to be sealed with BIOSTOP 500+ Intumescent Firestop Sealant to meet NEC #300.21, ASTM #E814, UL #1479 and UL #2079 standards.

Faces
Letters: White Lexan.
Arrow: White Lexan w/Blue vinyl graphics as shown Calon II 2870.

Trim Cap
Letters: 3/4" Blue Jewelite.
Arrow: 3/4" White Jewelite

Returns
Fab. .040 alum., 3" deep, .090 alum. backs.
Letters: Paint GripGuard Blue TM Calon II 2870, semi gloss finish.
Arrow: Pre-coat White, semi gloss finish.

Note: Paint inside letters and arrow AKZO Bright White reflective finish.

(2) Two Sets Required

PN:
13" Bldg Directional Letterset w/Arrow

Account: Rite Aid 6033
Project Title: 580 South State St Ukiah, CA
Date Created: 29, August, 06
Drawn By: J.Mullen
IP Rep: Ochoa
Approved By:
Date Revised: 31, August, 06

06-0829

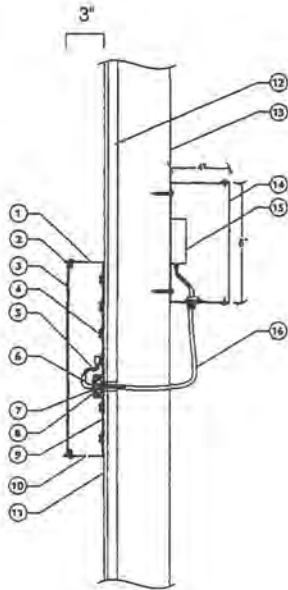
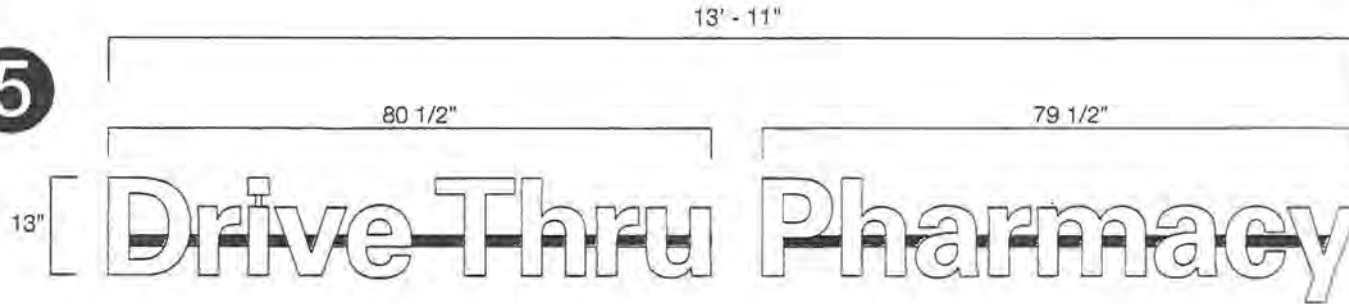
Pg 7 of 13

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5

Sign Area: 17 sf



- ① .040 Aluminum returns, 2 1/2" deep.
- ② 3/4" Jewelite trim cap.
- ③ White Lexan face.
- ④ Sloan white LEDs.
- ⑤ Low voltage wiring from power supply.
- ⑥ Pre-drilled electrical hole w/plastic grommet.
- ⑦ Aluminum raceway.
- ⑧ Anchors as req'd (min. 3 per raceway).
- ⑨ .090 Alum. letter back. Slides and backs stitched together and siliconed.
- ⑩ 1/4" Weep holes w/internal light baffle.
- ⑪ Alucabond facade by others.
- ⑫ Sheathing by others.
- ⑬ Vertical studs by others.
- ⑭ 4" x 8" x 18" JIC box.
- ⑮ Low voltage LED power supplies (120 volt circuit req'd).
- ⑯ High voltage rated flexible wire sleeves.

Illumination
Sloan White LEDs.

Wireways
Fabricated 1 1/2" x 1/2" deep alum., painted GripGuard Blue TM Calon II 2870, semi gloss finish. Note: Access to rear of facade to by others.

Note: Building dimensions and exact construction of decor band to be verified by sign installer prior to installation of signs. All penetrations to be sealed with BIOSTOP 500+ Intumescent Firestop Sealant to meet NEC #300.21, ASTM #EB14, UL #1479 and UL #2079 standards.

(1) One Set Required

Scale: 3/4" = 1'-0"

Faces
White Lexan.

Trim Cap
3/4" Blue Jewelite.

Returns
Fab. .040 alum., 3" deep, .090 alum. letter backs. Paint returns GripGuard Blue TM Calon II 2870, semi gloss finish.

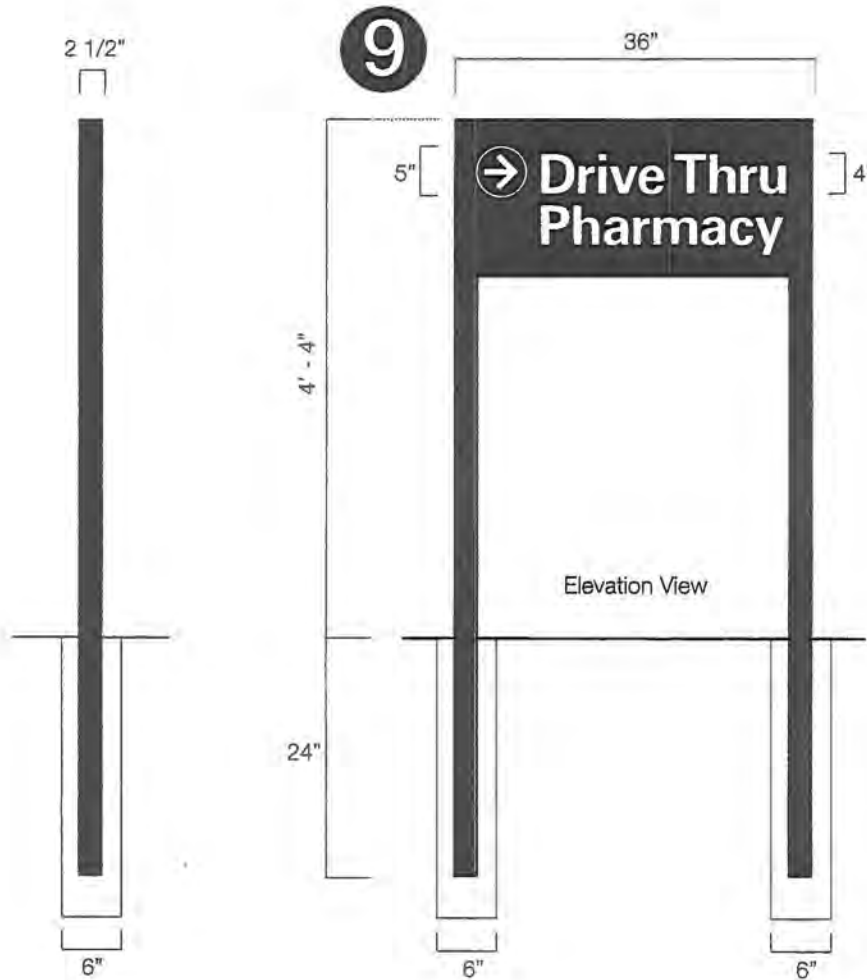
Note: Paint inside letters and arrow AKZO Bright White reflective finish.

PN:

13" Canopy Directional Letterset

Account: Rite Aid 6033
Project Title: 680 South State St Ukiah, CA
Date Created: 29, August, 06
Drawn By: J.Mullen
IP Rep: Ochoa
Approved By:
Date Revised:

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Back

Head
 Fab. .090 aluminum, paint GripGuard Blue TM Calon II 2870 (front and back). Semi gloss finish.

Copy
 Scotchlite Reflective White vinyl 480-10 (verify)

Posts
 2 1/2" Square alum. tube. Paint GripGuard Blue TM Calon II 2870. Semi gloss finish.
 Direct embed 2' into augured Sakrete footings.

Scale: 1" = 1'-0"

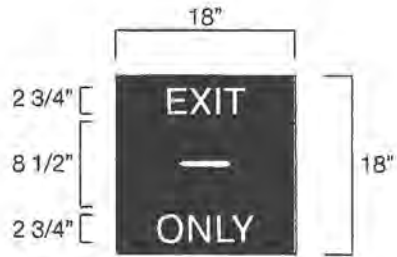
Sign Area: 4 sf

(1) One Sign Required

PN
 Non Illuminated DF Directional

Account Rite Aid 6033 Drawn By J.Mullen
 Project Title 680 South State St IP Rep. Ochoa
 Ukiah, CA Approved By
 Date Created 29, August, 06 Date Revised

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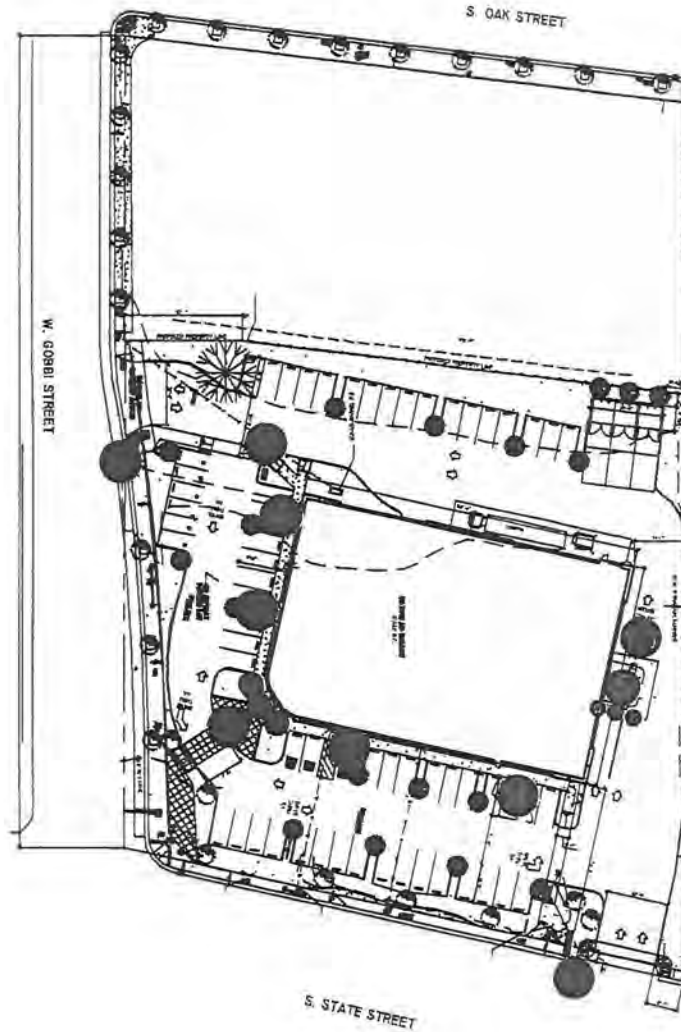
10

SF Traffic Control Sign

Bkgd
 .125 Alum., paint GripGuard Blue
 TM Calon II 2870 (front and back).
 Semi gloss finish.

Copy
 Scotchlite Reflective White vinyl
 3M 480-10 (verify).

Symbol
 Red Reflect vinyl 3M 480-82
 w/White Reflective vinyl detail
 3M 480-10 (verify).

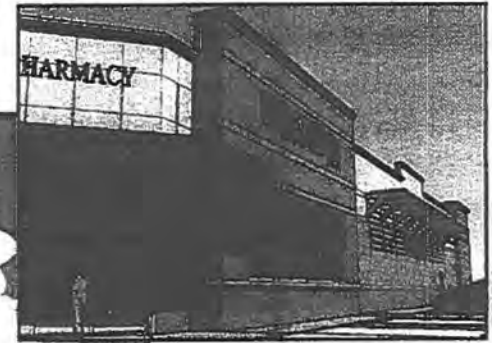
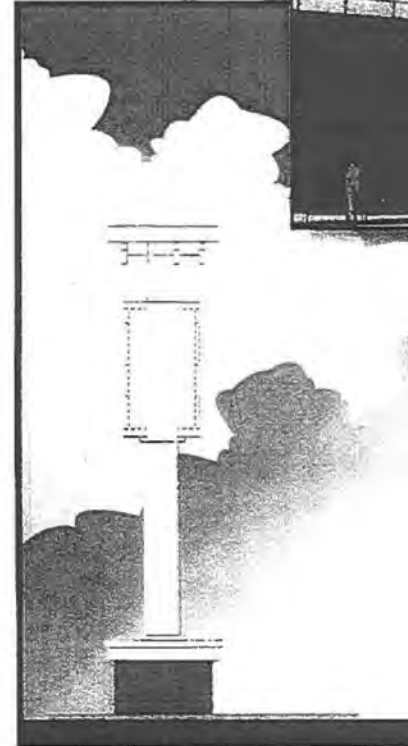
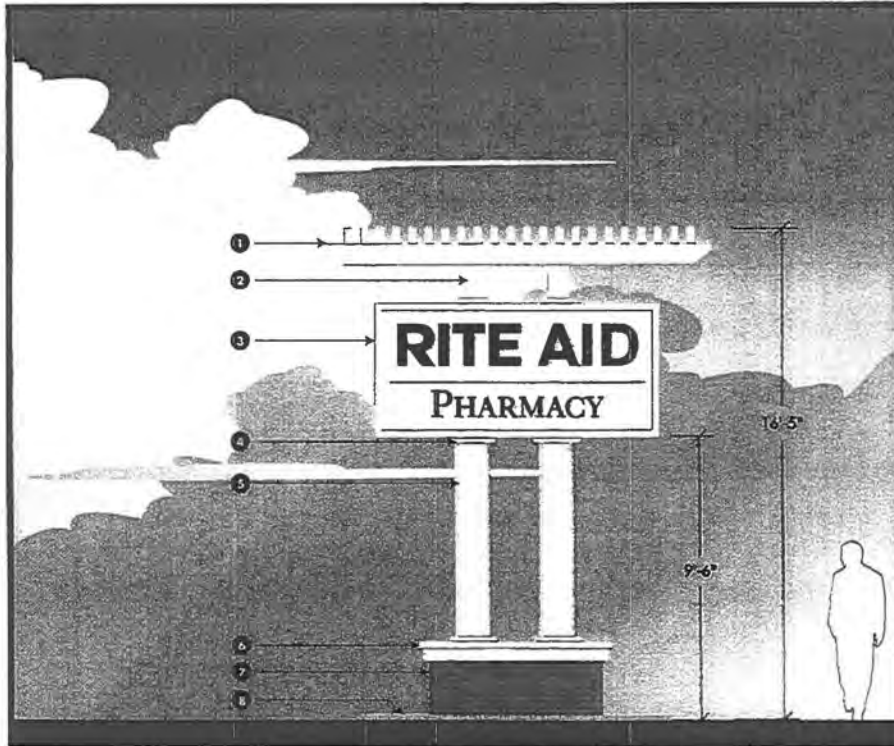


Account Rite Aid 6033 Drawn By J.Mullen
 Project Title 680 South State St. Ukiah, CA IP Rep. Ochoa
 Approved By
 Date Created 29, August, 06 Date Revised

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11

- 1 ALUMINUM FABRICATED TRELLIS POWDER COATED OFF-WHITE TO MATCH BUILDING TRELLIS DETAIL.
- 2 8" ALUMINUM SQUARE POST POWDER COATED OFF-WHITE TO MATCH BUILDING TRELLIS DETAIL
- 3 3" ALUMINUM FRAME FABRICATED AROUND FORMED POLYCARBONATE FACES & INTERNALLY ILLUMINATED. CABINET COLORS POWDER COATED TO MATCH BUILDING COLORS & LETTER COLOR TO MATCH CORPORATE STANDARD.
- 4 2" ALUMINUM RINGS POWDER COATED OFF-WHITE
- 5 1 1/2" ALUMINUM ROUND POLE POWDER COATED METALLIC SILVER TO MATCH WINDOW TRIM.
- 6 ALUMINUM FABRICATED CAP POWDER COATED OFF-WHITE.
- 7 BRICK BASE TO MATCH BRICK OF BUILDING.
- 8 CONCRETE FOOTING.



PROPOSED BUILDING

PARAMOUNT	MICHAEL COV	PROJECT	DESCRIPTION	PHASE	SCALE	DATE REVISIONS	PAGE
-----------	-------------	---------	-------------	-------	-------	----------------	------

2701 Road 1
Redwood Valley CA 95470
707.485.7555
707.485.5081 (Fax)
Lic. 636883

1714 EL CAMINO WAY
SANTA ROSA CA 95404
707.568.0398
mcdesign@comcast.net

RITE AID

POLE SIGN
OPTION B

FINAL

1/4" = 1'-0"

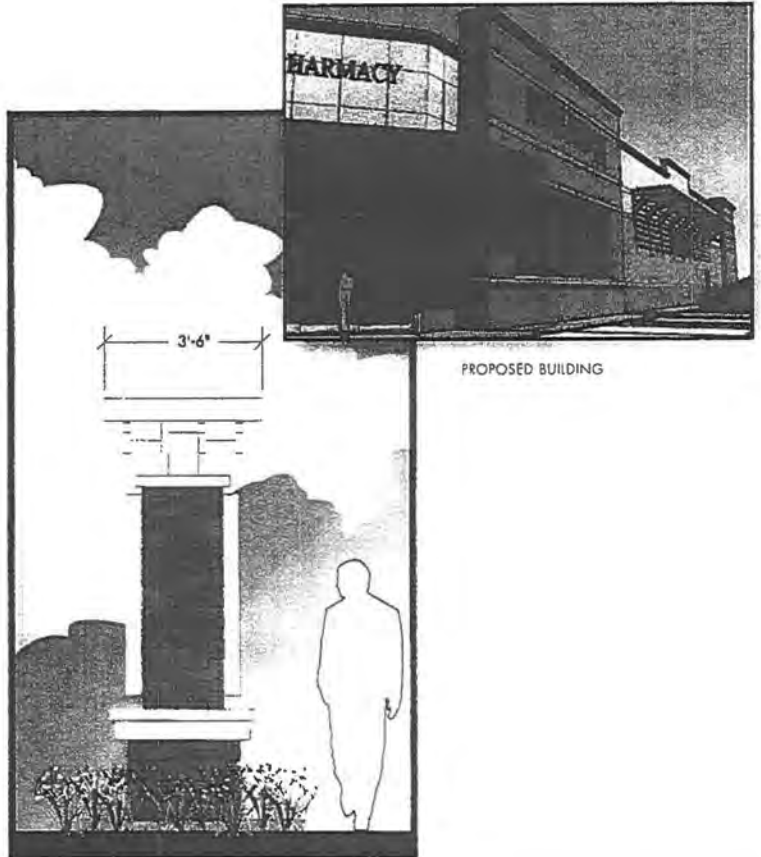
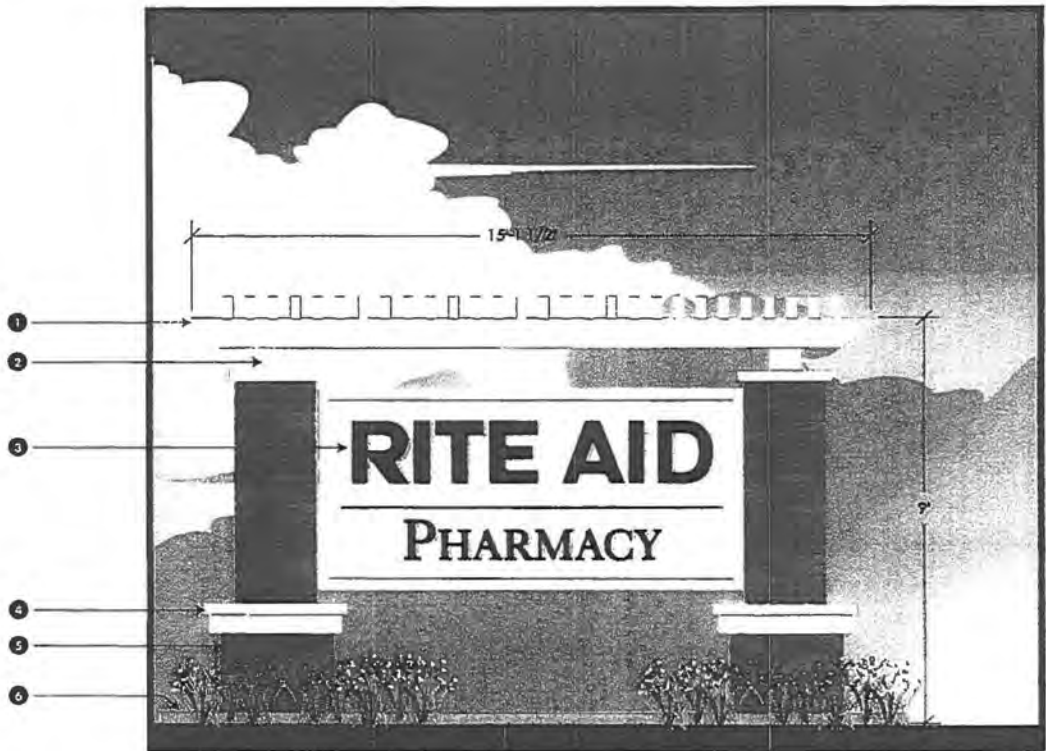
12.1.2006

1.1

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12

- 1 ALUMINUM FABRICATED TRELLIS POWDER COATED OFF-WHITE TO MATCH BUILDING TRELLIS DETAIL.
- 2 8" ALUMINUM POST POWDER COATED OFF-WHITE TO MATCH BUILDING TRELLIS DETAIL.
- 3 3" ALUMINUM FRAME FABRICATED AROUND FORMED POLYCARBONATE FACES & INTERNALLY ILLUMINATED. CABINET COLORS POWDER COATED TO MATCH BUILDING COLORS & LETTER COLOR TO MATCH CORPORATE STANDARD.
- 4 ALUMINUM FABRICATED CAP POWDER COATED OFF-WHITE.
- 5 BRICK BASE & COLUMN TO MATCH BRICK OF BUILDING.
- 6 CONCRETE FOOTING.



PARAMOUNT	MICHAEL COX	PROJECT	DESCRIPTION	PHASE	SCALE	DATE REVISIONS	PAGE
2701 Road 1 Redwood Valley CA 95470 707.485.7555 707.485.5081 Fax Uc. 636883	1714 EL CAMINO WAY SANTA ROSA CA 95404 707.568.0398 mcdesign@comcast.net	RITE AID	MONUMENT SIGN OPTION A	FINAL	3/8"=1'-0"	12.1.2006	2.0

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MITIGATED NEGATIVE DECLARATION/ INITIAL ENVIRONMENTAL STUDY

Project Title: RITE-AID DRUGSTORE
Major Site Development Permit No. 06-19 and
Minor Subdivision Map No. 06-36

Lead Agency Name and Address: City of Ukiah Department of Planning and
Community Development
300 Seminary Avenue
Ukiah, CA 95482

Project Location: 680 South State Street (APN 002-301-43)
City of Ukiah, Mendocino County, CA

Project Sponsor's Name and Address: Ruff & Associates, Inc.

General Plan Designation(s): C (Commercial)

Zoning: C-1 (Commercial)

Contact Person: Dave Lohse, Associate Planner

Phone Number: 707-462-6206

Date Prepared: January 18-22

Public Review Period: January 23-February 12, 2007

PROJECT DESCRIPTION

Existing Conditions: The subject property consists of a single 2.5-acre parcel that extends from the west side of South State Street to the east side of South Oak Street, with a third frontage along West Gobbi Street. The lot is fully developed with the existing 31,500 square foot Rite-Aid drugstore, with the west-facing wall of the retail store abutting the sidewalk for South Oak Street and its customer entrance facing the large asphalt parking lot that covers the eastern portion of the site. This store is approximately 18 feet above grade, with a flat roof over most of the structure and a mansard roof cover over the customer entrance area, which consists of glass entry doors and large-pane windows. Walls are solid masonry inset with rock, with no windows or customer entrances on the north, west, or south sides.

The eastern portion of the site is developed with a large parking area that has approximately 154 parking stalls, with perimeter landscape planters along the eastern and southern sides. These planters do not have extensive landscaping and there are only a few shade trees in smaller planters at the ends of long rows of parking. State Street access is available via a 36-foot wide driveway at the northeast corner of the lot, with additional available via a 31-foot wide driveway near the center of Gobbi Street frontage and a 30-foot wide driveway along Oak Street, near its intersection with Gobbi Street. The area south of the building is used for truck access to the loading dock on the southwest corner of the building.

As noted above, the project site has been almost entirely developed, with building foundations or asphalt parking/access roadways covering at least 90-95 percent of its surface area. This area is relatively flat, with drainage running from west to east.

Proposed Conditions: The applicants have filed two applications to develop the new Rite-Aid drugstore, as described below.

- ❖ Minor Subdivision Map No. 06-18: The approval of this application would divide the 2.5-acre parcels into proposed Parcel One of 68,488 square feet and Parcel Two with 38,001 square feet, respectively. The existing Rite-Aid store would be located on Parcel Two, with the proposed boundary line running in a north-south direction approximately 5-10 feet east of its eastern wall. The new store would be constructed on Parcel One, which consists of the existing parking lot.
- ❖ Major Site Development No. 06-19: The approval of this project component would allow the actual development of the proposed retail drugstore on the eastern portion of the site. This commercial development would consist of the following:

- The construction of a 17,422 square foot drugstore building in the center of proposed Parcel One. This building would parallel the State Street frontage, with its east-facing wall approximately 80 feet from the back edge of the sidewalk that fronts this street and the south side of the building 50-70 feet from the Gobbi Street right-of-way.

This building would be 23-25 feet high, with a flat roof obscured by a segmented parapet roof treatment along all 4 sides. The building would be generally rectangular in shape, with the southeast corner cut off at an angle to provide a glassed-in corner entry for customers. There would also be large pane windows on the south and east walls, abutting the corner entry, and smaller-pane windows along near the middle part of the east wall and the west wall. There would also be smaller pane windows on the "drive-thru pharmacy" proposed for the northern wall of the building.

Building walls would be constructed with concrete masonry blocks covered by an attractive mix of masonry and metal materials. These include outset walls of "bronze-tone" brick on either side of the customer entry area and on the other corners of the building. The inset walls between the brick columns would consist of lighter "golden-buff" brick approximately 13.5 feet from grade, with white scored stucco covering the walls above this brick. The walls would also utilize cast-stone moldings on the bricked areas and stucco covered foam trim on the stucco walls. Additional wall accents would include white metal trellises on the inset walls on the building's east and west sides, and a bright blue metal panel over the building entrance.

The north side of the building would also have a 23-foot wide by 30-foot long canopy roof over the pharmacy drive-up area. This flat-roofed structure would be constructed with masonry materials covered by white and blue metal panels similar to those use on other parts of the building.

- The areas surrounding the building would be used for paved access and parking or landscaping. The project site would utilize the same driveway locations as the existing store, but the driveways along State Street and Gobbi Street frontages would both be reduced to 30-foot widths. These driveways would provide ingress and egress to access lanes that will circle the building, providing through access to persons using the drive-thru pharmacy and to persons who park on the site.

The most accessible parking will be in two rows of stalls along the east and south sides of the building, with 6 compact and 9 full-size stalls to the south and 3 handicapped parking stalls and 26 full-size stalls to the east. There are also 17 full-size stalls proposed for the west side of the building, but these stalls are not as accessible to the store entrance and will require users to walk through the loading dock area that would be developed on the west side of the store. The area west of the store would also be used for a 430 square foot trash/recycle area that would be constructed with masonry materials similar to the building.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

- The remaining areas of the site will be used to widen the existing linear planter areas along the east and south perimeters of the site and add new linear planters along the western perimeter, with smaller planter areas in the northeast and northwest corners of the site. There would also be a greater number of shade trees planted in planter wells between parking stalls.

The conceptual landscape plans submitted with the project shows that the landscaped areas would be planted with groundcover, shrubs, lawn, and trees, including deciduous Maple tree species (*Acer Rubrum* October Glory) and European Hornbeam species (*Carpinus betulus*) in the perimeter planters along the east and south sides of the building and in the planters located in the parking areas. Three evergreen tree species (*Cuppiessus semervirens* 'glauca') would be planted in the widened planter area located along the southwest corner of proposed Parcel One.

The landscape plan also includes dedicated pedestrian paths between the store entrance and the intersecting sidewalks along State and Gobbi Streets, with an unspecified paving that would be different than asphalt.

Surrounding Land Uses and Setting: The subject properties are located in an urbanized area of Ukiah that has been developed with retail and professional offices to the south, the Safeway supermarket to the east, a motel to the north and low- to high-density residential to the west.

Other Public Agencies With Approval Authority (e.g., permits, financing approval, or participation agreement):

- Mendocino County Air Quality Management District
- City of Ukiah Building Department
- City of Ukiah Department of Water and Sewer
- City of Ukiah City Engineer/Department of Public Works
- City of Ukiah Electrical Department

Environmental Factors Potentially Affected:

The environmental factors checked below would be potentially affected by this project, as indicated by the checklist and corresponding discussion on the following pages.

- | | | |
|---|---|--|
| <input checked="" type="checkbox"/> Aesthetics | <input type="checkbox"/> Agricultural Resources | <input checked="" type="checkbox"/> Air Quality |
| <input checked="" type="checkbox"/> Biological Resources | <input checked="" type="checkbox"/> Cultural Resources | <input checked="" type="checkbox"/> Geology / Soils |
| <input checked="" type="checkbox"/> Hazards & Hazardous Materials | <input checked="" type="checkbox"/> Hydrology / Water Quality | <input checked="" type="checkbox"/> Land Use / Planning |
| <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Population / Housing |
| <input checked="" type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input checked="" type="checkbox"/> Transportation / Traffic |
| <input checked="" type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.
- I find that although the proposed project could have a significant effect on the environment there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A **MITIGATED NEGATIVE DECLARATION** will be prepared.
- I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the proposed project **MAY** have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or **NEGATIVE DECLARATION** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or **NEGATIVE DECLARATION**, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Planner's Signature:

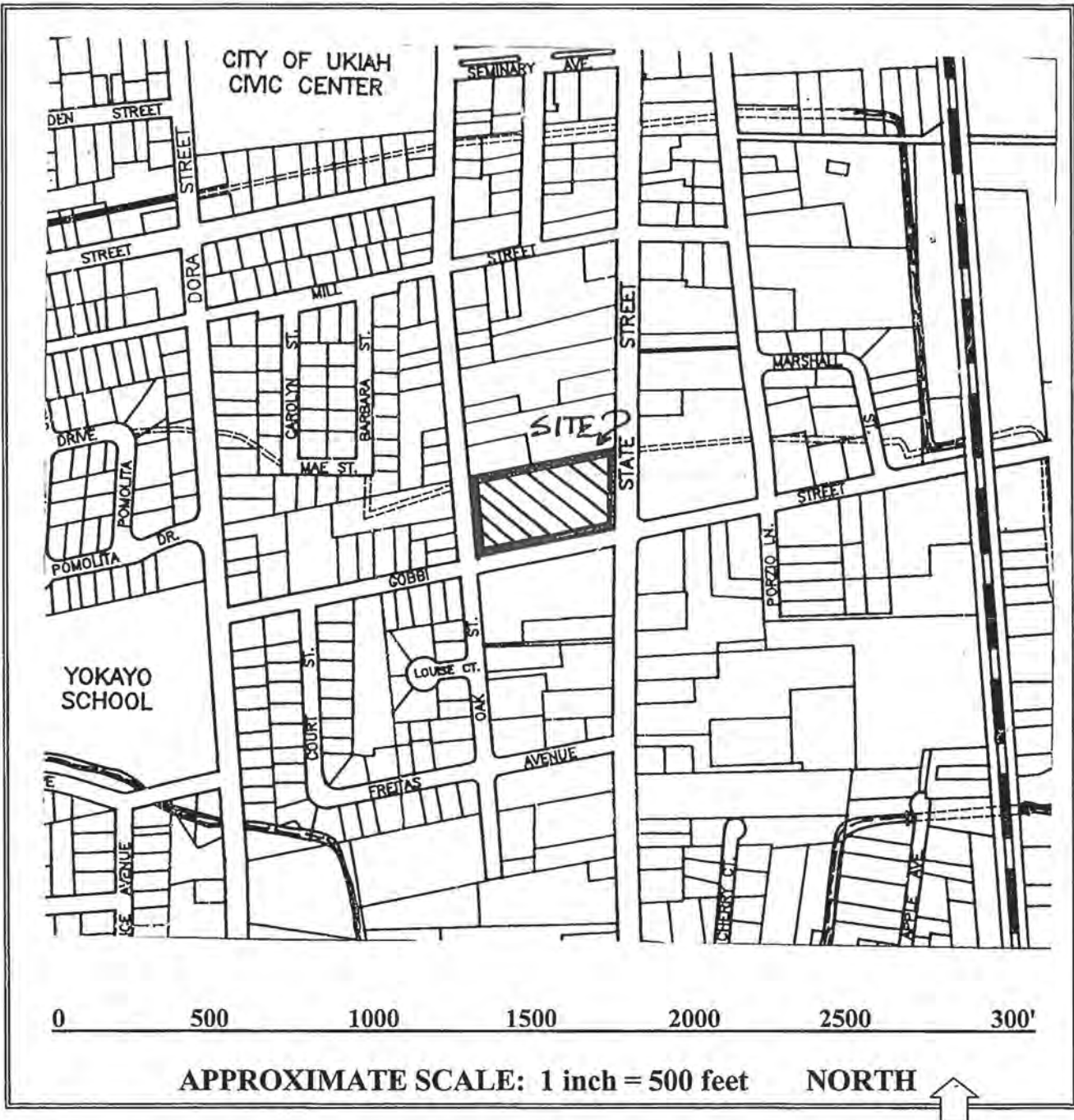
Date: January 22, 2006

Planner's Printed Name Charley Stump

Director, City of Ukiah Department of Planning & Community Development

LOCATION MAP

Rite-Aid Drugstore Minor Subdivision and Major Site Development Permit
MS 06-18 & SDP 06-19
680 South State Street



INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

PURPOSE OF THIS INITIAL STUDY

This Initial Study has been prepared consistent with CEQA Guidelines Section 15063, to determine if the project, as proposed, may have a significant effect upon the environment. Based upon the findings contained within this report, the Initial Study will be used in support of the preparation of Mitigated Negative Declaration.

I. AESTHETICS		Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:					
a)	Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b)	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c)	Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d)	Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion

Building Impacts: The development of the proposed drugstore will definitely change the visual quality of the site, which now consists of a large parking lot. In this case, staff anticipates that the proposed building design contains sufficient design features to present an interesting appearance that is consistent with the City's commercial design guidelines and is compatible enough with surrounding structures to fit in with the aesthetic characteristics of the neighborhood. It also has a more attractive and efficient landscape plan than the existing store and its grounds. Based on these factors, staff anticipates that the new structure will present a more interesting and attractive alternative than the existing structure and its parking lot, and does not recommend that any measures to lessen the impact of its site or building design be required as a mitigation measure.

However, staff notes that the existing structure may have to be demolished before the proposed drugstore can be constructed since the new building would be located in the parking lot for the existing structure. Planning staff is concerned that the demolition of this building and the retention of building pads, asphalt surfaces, or bare ground could cause a potentially significant adverse visual impact at the site and to the surrounding neighborhood. Therefore, staff recommends that the applicants be required to remove all building debris and cover all exposed land surfaces with landscaping (such as lawns) or other surfacing that will reduce the potential for a blighted appearance from the removal of the structure. This plan should include a detailed schedule for the demolition of the building, the removal and/or recycling of building debris, and the covering of exposed areas.

Signs: The applicants have submitted a conceptual sign program for the new store and staff has determined that this plan contains well-designed signage that is generally consistent with the sign code provisions of the Ukiah Municipal Code. However, staff noted the program includes the retention of an existing pole sign and the proposed use of additional monument signs along both the State Street and Gobbi Street frontages, resulting in three freestanding signs on the site. The UMC clearly limits commercial developments to a single freestanding sign and the construction of two new monument signs

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will require the approval of a variance. Therefore, Planning staff recommends that the number of signs is reduced to lessen the adverse aesthetic impact caused by this number of signs, with a single lower sign constructed near the Gobbi Street/State Street intersection to provide maximum exposure.

The proposed sign program also includes some extraneous signs that are not particularly consistent with the commercial design guidelines that apply to this site particularly the smaller signs on the metal panel above the main building entrance that advertise photo, pharmacy, and food mart uses. However, while the aesthetic impacts of these smaller signs are highly subjective, they do not cause and adverse aesthetic impact that warrants mitigation.

Light Impacts: The proposed building would have several large glass windows and doors oriented to the east and south, which could cause glare during some parts of the day. However, staff notes that the largest panes would be partially shaded by trellises or a canopy over the front door, and that the shade trees proposed for the parking stalls abutting the building will provide additional shade and some screening. Furthermore, the glass will not be directly oriented toward any nearby structures or public streets, so staff does not anticipate any substantial impacts from glare.

Project site plans show at least ten pole lights are proposed for placement around the perimeter areas of the site, but the applicants have not yet chosen a specific style or size of light pole and there is no detailed information on the direction or range of the light that will be broadcast by these lights. Additionally, building elevations do not show any building lights. The subject property is on a highly visible corner lot and the potential for high nighttime lighting levels to cause adverse visual impacts is therefore considered significant in this case. However, it is the opinion of staff that these impacts would not be significant if lighting methods and equipment recommended by the International Dark Sky Association are used to limit lighting levels to those that are necessary for security only. These "dark sky" measures are included in the mitigation measures below.

Mitigation Measures: The following measures are required to ensure that adverse visual impacts caused by building demolition, signage, and exterior lighting are reduced to levels that are not significant.

1. A schedule with detailed information regarding the demolition of the existing drugstore building, the disposal and/or recycling of demolition materials, and the methods and timing for covering all exposed areas shall be submitted to the Ukiah Department of Planning and Community Development for review and shall be approved by its staff prior to the issuance of any demolition permits or for any site preparation or construction on either of the proposed parcels.
2. The number and size of freestanding and building signs shall be restricted to those prescribed by the Sign Code provisions of the Ukiah Municipal Code or permitted in a final sign program approved by the Ukiah Planning Commission as part of the discretionary permit approval.
3. Prior to the issuance of any Building Permit for the construction of the retail store building, a Final Lighting Plan shall be submitted to the Director of Planning and Community Development or his/her designee for review for compliance with Ukiah Municipal Code standards for on-site lighting and with "dark sky" guidelines for reducing nighttime lighting on the site. The Final Lighting Plan shall include details regarding exterior lighting for structures, garden areas, and walkways, with lighting sources that are full cut-off, hooded, and down-cast, or otherwise shielded to ensure that light does not adversely shine towards neighboring properties, or toward the night sky. Additionally, all lighting shall be the minimum wattage necessary to provide adequate security, yet shall not result in excessively bright night glow. Sufficient details regarding the proposed wattage of all site lights shall be included in the Final Lighting Plan so that the Planning Staff can determine how bright the proposed site lights will be. The Director of Planning and Community Development shall have the authority to require the Final Lighting Plan to be modified (including the wattage) and/or additional information to be submitted so that the lighting meets the requirements listed above.

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Impact Significance After Mitigation: Based upon the comprehensive nature of the recommended mitigation measures, staff is able to conclude that the project, as mitigated, will not cause or substantially contribute to adverse aesthetic impacts.

2. AGRICULTURAL RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland.

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The proposed project would allow for the subdivision of a previously developed site and the construction of a commercial building on a property zoned for commercial uses. No agricultural operations occur on the project site, which is in an urbanized area of the City that is zoned for commercial uses. The project site is not in an agricultural preserve.

The California Natural Resources Conservation Service soils information for Mendocino County indicates that the site is located in an area where soils consist of an "urban mix", with no indication of the presence of prime farm land at this location. There is no actively farmed prime agricultural land adjacent to this site or within one-half mile of the site, and the development of the drugstore use would not result in the conversion of farm land to non-agricultural uses, either directly or indirectly. (a, b, c)

3. AIR QUALITY

Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion: The City of Ukiah is situated in the flat and narrow Ukiah Valley and the presence of the mountains on both the west and east sides of the valley tends to restrict the horizontal east-west movement of pollutants. While the potential for air pollution is high in the Ukiah Valley, measurements provided by the Mendocino County Air Quality Management District (MCAQMD) indicate that actual pollutant levels are relatively low due to the lack of upwind sources and the relatively low level of development in the local air basin.

The MCAQMD reviewed the subject proposal. In a letter dated 5/30/06, the MCAQMD commented as follows: 1) Asbestos Demolition – the District enforces federal regulations associated with this issue. Demolition of the existing structure will require asbestos clearance and notification to the District. The Health and Safety Code requires that building departments cannot issue demolition permits until they have received clearance from the District; 2) Any stationary on-site internal combustion engines over 50 horsepower (i.e. generators) may require a permit from the District depending upon fuel source and level of operation; 3) bus stops should be installed if deemed necessary based upon MTA’s project recommendation; 4) the District has requested additional information regarding an evaluation of vehicle emissions generated by the Rite Aid drive-thru facility; 5) the District recommends that a use permit condition be included which limits the use of the pharmacy drive-thru feature to pharmacy customers only (i.e. no general retail customers allowed); all grading activities must comply with District Regulation 1 Rule 430 regarding fugitive dust emissions; 6) the District recommends that the landscape plan include adequate shade trees in the proposed parking areas.

Based on the comments/recommended mitigation measures received from the MCAQMD, the project would not conflict with applicable air quality plans affecting the area, and would not be expected to violate any air quality standards or result in a cumulatively considerable net increase of pollutants.

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The primary odor/dust/emissions receptors of concern are the occupants of the residences and businesses located in the immediate vicinity of the project site. The proposed project will result in short-term impacts associated with construction activities, including grading activities. As noted above, MCAQMD recommends implementing adequate dust suppression methods during grading and construction activities to minimize the generation of fugitive dust. Methods may include the use of palliatives, frequent watering, or other methods approved by MCAQMD.

Odors generated by on-site activities can be reduced by regular trash collection, storage of food wastes in closed containers, and enclosure and containment of outside storage materials. Mitigation measures and permit conditions would ensure less than significant levels of air quality impacts.

Mitigation Measures:

4. Construction activities shall be conducted with adequate dust suppression methods, including watering during grading and construction activities to limit the generation of fugitive dust or other methods approved by the Mendocino County Air Quality Management District. Prior to initiating soil removing activities for construction purposes, the applicant shall pre-wet affected areas with at least 0.5 gallons of water per square yard of ground area to control dust.
5. The burning of construction debris is prohibited. Any disposal of vegetation removed as a result of lot clearing shall be lawfully disposed of, preferably by chipping and composting, or as authorized by the Mendocino County Air Quality Management District.
6. During construction activities, the applicant/owner/contractor shall remove daily accumulation of mud and dirt on paved access lanes that serve the project site.
7. The applicant/owner shall secure a grading permit from the Planning and Community Development Department and shall adhere to all grading permit conditions, including Best Management Practices. All areas disturbed by grading shall be either paved with concrete or asphaltic concrete, landscaping or erosion control hydroseed mixture.
8. All refuse generated by the facility shall be stored in approved disposal/storage containers, and appropriately covered. Removal of waste shall be on a weekly basis at a minimum so as to avoid excess waste. All trash receptacles/containers shall remain covered at all times to prevent fugitive odors and rodent infestation.
9. Demolition of the existing Rite Aid structure will require asbestos clearance and notification to the Mendocino County Air Quality Management District. The City of Ukiah Building Division cannot issue a demolition permit until they have received clearance from the MCAQMD.
10. Any stationary on-site internal combustion engines over 50 horsepower (i.e. generators) may require a permit from the MCAQMD depending upon fuel source and level of operation.
11. The use of the pharmacy drive-thru feature shall be limited to pharmacy customers only (i.e. no general retail customers allowed). Prior to use of the drive-thru feature, the applicant must demonstrate to the MCAQMD that vehicle emissions associated with the drive-thru feature will not contribute significantly to air quality in this area or the valley in general.
12. All grading activities must comply with MCAQMD Regulation 1 Rule 430 regarding fugitive dust emissions

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Impact Significance After Mitigation: Based upon the comprehensive nature of the recommended mitigation measures, staff is able to conclude that the project, as mitigated, will not cause or substantially contribute to an existing or projected violation of State PM-10 standards or soil erosion.

4. BIOLOGICAL RESOURCES	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The subject property is located in an older, urbanized area of Ukiah. The site has been completely developed with a structure and paved parking area. There are several medium size ornamental landscape trees and shrubs which exist as part of the landscaping features associated with the existing Rite Aid store, but there are no known endangered, threatened or sensitive plant species on the site.

It is not anticipated that the subject minor subdivision and/or Rite Aid development proposal at this location would hinder the movement of animals, nor significantly intrude on their habitat as no natural habitat exists at this location and in the general vicinity. Removal of the existing ornamental on-site trees and shrubs will cause minimal short term impacts to bird habitat in this area. However, the existing vegetation does not support extensive populations of any known rare or endangered bird species. The

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site will be replanted consistent with City code associated with landscape requirements, which will provide similar or improved habitat as compared to that which currently exists. (See recommended mitigation under the Initial Study *Aesthetics* section). Due to the extensive amount of existing development of this site, and lack of any support in the record to the contrary, staff has determined that the project qualifies for the *de minimus* exemption pursuant to the Department of Fish and Game regulations. No additional mitigation is required.

(a, b, c, d, e, f)

5. CULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Cause a substantial adverse change in the significance of a historical resource as defined in 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to 15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: The City of Ukiah is rich in historical resources including an eclectic assortment of historic homes and commercial structures. Cultural resources are known to exist throughout the City, and the City has provided for the preservation and enhancement of its cultural heritage. The existing Rite Aid store, which is proposed to be demolished, is not listed in any local or other resources associated with architectural or historic values that require preservation. A review of the minutes from the Planning Commission hearing of February 22, 2006 did not reveal any indication that the existing structure was historic or an example of unique architecture which should be preserved. Consideration of historic design features in the vicinity has been considered in association with the design and construction of the proposed structure (see the *Aesthetics* section of this report).

The project site has been nearly completely developed with a commercial structure and associated paved parking lot. It is not anticipated that the proposed project will cause a substantial adverse change in archaeological or historical resources of the subject site or the surrounding area. However, there is always the possibility that unknown cultural resources may be uncovered during construction activities.

The project site contains no known paleontological resources or unique geologic sites. No human remains are likely to be encountered, as the project site has not been known to be used as a burial ground. Section 7050.5 of the California Health and Safety Code outlines procedures to be followed in the event that any human remains are found. (a, b, c, d)

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Mitigation Measures:

13. In the event that archaeological remains or artifacts are uncovered during construction activities, work shall be stopped and a qualified archaeologist shall survey the site with all costs borne by the applicant/owner. The archaeologist shall submit a report to the City Planning and Community Development Department with recommendations on the disposition of the site. Disposition may include, but is not limited to, excavation and documentation, capping the site, or leaving the site in an open space area. The recommendations of the archaeologist shall be incorporated in the project as project mitigation.

6. GEOLOGY AND SOILS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Discussion: The Ukiah Valley is part of an active seismic region that contains the Maacama Fault, which traverses the valley to the east and north of the City limits. According to resource materials maintained by the Ukiah Planning and Community Development Department, the projected maximum credible earthquake along this fault would be approximately 7.4 magnitude on the Richter scale. The subject site is not situated on or near an Alquist-Priolo fault zone and has no known slope and/or foundation instability, soil instability, or other geologic hazards.

All buildings would be subject to the applicable provisions of the Uniform Building Code (UBC) and the California Building Code (CBC) as determined by the Building Division of the Planning and Community Development Department.

Grading activities associated with the project would loosen soils, exposing them to potential water and wind erosion. However, mitigation measures described in Section 3 (Air Quality) and Section 8 (Hydrology and Water Quality) would reduce potential erosion problems. With completion of construction and landscaping, the potential erosion hazard on the project site would be minimal. (a, b, c)

The Soil Survey of Mendocino County, Eastern Part, and Trinity County Southwestern Part prepared by the U.S. Soil Conservation Service, indicates the site is underlain by an "urban mix" of soils that includes native soils mixed with non-native fill materials. Currently, approximately 80% of the on-site soils are covered with asphalt parking areas and structures. This percentage will be approximately the same after completion of the proposed project. (d, e)

Mitigation Measures:

- 14. The proposed development shall be designed and constructed in accordance with the most current applicable Building Codes, including the Uniform Building Code (UBC) and the California Building Code (CBC) as determined by the Building Division of the Planning and Community Development Department of the City of Ukiah.

Table with 5 columns: 7. HAZARDS AND HAZARDOUS MATERIALS, Potentially Significant Impact, Less Than Significant With Mitigation Incorporated, Less Than Significant Impact, No Impact. Rows include questions about hazardous materials and emissions.

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7. HAZARDS AND HAZARDOUS MATERIALS

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: Ukiah is generally regarded as a healthy city with relatively clean air and water. While there are some known toxic "spots" resulting from the past storage of hazardous materials underground, the City is not regarded as having a highly contaminated environment. Based upon a field review, as well as a review of contaminated sites maintained by the City and other agencies, it has been determined that the site is in a clean and healthy condition and is not contaminated with toxic or hazardous materials that would present a significant health hazard.

An issue of concern is worker health and safety and public exposure to hazardous materials during construction. These hazards are typically related to construction-related fuel spills. The proposed project would not require long-term storage, treatment, disposal, or transport of significant quantities of hazardous materials. Only small quantities of petroleum hydrocarbons and their derivatives (e.g. gasoline, diesel fuels, oils, lubricants, and solvents) required to operate the construction equipment would be stored, used, and handled during project construction. These relatively small quantities associated with the construction are generally below reporting requirements for hazardous materials business plans. The project applicant would be required to prepare a Hazardous Materials Business Plan to be submitted for review and acceptance by the Planning and Community Development Department if the storage capacity equals or exceeds 55 gallons. If the capacity is less than 55 gallons, the project applicant would prepare a Hazardous Materials/Waste registration form.

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Implementation of the following measures would reduce potentially significant impacts to less than significant levels and have been incorporated into the mitigation measures recommended below:

- ❖ Equipment fueling and service shall be conducted at a designated location other than the subject site, including at local gas stations or repair shops;
- ❖ Any spills resulting from fueling or hydraulic line breaks will be contained and cleaned up immediately;
- ❖ During construction activities, equipment shall be maintained and kept in good operating conditions to reduce the likelihood of line breaks and leakage;
- ❖ No refueling or servicing shall be done without absorbent materials (i.e. absorbent pads, mats, socks, pillows, and granules) or drip pans underneath to contain spilled materials; and
- ❖ If contaminated soils are encountered during construction, proper storage and/or disposal of any contaminated soils that meet the definition of a hazardous waste shall be provided, and that such soils are removed for off-site treatment or disposal at an appropriate landfill.

The project site is located approximately 3/4 of a mile NW of the Ukiah Municipal Airport. The subject parcel lies within the Airport Comprehensive Land Use Plan (CLUP) Zone C which lists *Intensive Retail Uses* as a *normally acceptable use* within this zone. Zone C is an area which is characterized as having *limited risk with frequent noise intrusion* pursuant to Table 2A of the adopted CLUP. The project would not interfere with an adopted emergency response or evacuation plan, as roads in the area would remain open.

Proposed mitigation measures and conditions of approval will ensure a less than significant level of impact associated with the site relative to hazards. (a, b, c, d, e, f, g, h)

Mitigation Measures:

15. Should the proposed use of the site require the storage of materials equal or greater than 55 gallons of a liquid, 500 pounds of a solid or 200 cubic feet of compressed gas, the applicant will be required to submit a Hazardous Materials Business Plan to the Planning and Community Development Department with said plan to be renewed and updated annually. If quantities are less than the amounts noted above, the applicant must submit a Hazardous Materials/Waste registration form to the Department.
16. Equipment fueling and service shall be conducted at a designated location other than the project site, including local gas stations or repair shops.
17. Any spills resulting from fueling or hydraulic line breaks will be contained and cleaned up immediately.
18. During construction activities, equipment shall be maintained and kept in good operating conditions to reduce the likelihood of line breaks and leakage.
19. No refueling or servicing shall be done without absorbent materials (i.e. absorbent pads, mats, socks, pillows, and granules) or drip pans underneath to contain spilled materials.

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8. HYDROLOGY AND WATER QUALITY

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (Source:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Discussion: Three major creeks flow through the City of Ukiah and drain into the Russian River. Some of the areas adjacent to the creeks are identified by the Federal Emergency Management Agency (FEMA) as being potentially subject to flooding events, but the Flood Insurance Rate Maps (FIRM Map Number 060186 0001E, effective date August 5, 1985) prepared by the Federal Emergency Management Agency reveal that this site is not located within any floodplain or floodway and is not subject to flooding events. The subject property is not in the floodplain, and is not anticipated to be subject to flooding. Domestic water quality, as well as the quality of creek waters in the City is rated as very good and will not be significantly degraded as a result of this proposal given adequate project mitigation as described within this report. Domestic water is available for the proposed project via the public City water system.

Existing absorption rates, drainage patterns and runoff volumes may be affected by this project, however, these patterns should not be significantly altered as the site has been substantially developed with impervious surfaces with drainage incorporated into the existing drainage system. The Ukiah City Engineer has reviewed the proposal and has recommended the preparation of an on-site Grading and Drainage Plan, an Erosion and Sediment Transport Control Plan, and drainage calculations showing that the proposed drainage facilities are adequate. No drainage is allowed to be routed over sidewalks. All reports and plans shall be prepared by a Registered Civil Engineer for approval by the City Engineer prior to the issuance of a building permit. Storm drain inlets must be protected by a fossil fuel filtering device if required by the City Engineer.

The project would not violate nor be inconsistent with Federal, State or local water quality standards and waste discharge requirements, as it would use the City's water supply and wastewater systems. This project would not have a significant impact on ground water supplies or quality, as the project would not utilize private wells. (a, b, c, d, e, f)

Mitigation Measures:

20. Prior to the issuance of building permits, the preparation of an on-site Grading and Drainage Plan, an Erosion and Sediment Transport Control Plan, and drainage calculations showing that the proposed drainage facilities are adequate shall be prepared by a Registered Civil Engineer for approval by the City Engineer. No drainage is allowed to be routed over sidewalks. Storm drain inlets must be protected by a fossil fuel filtering device if required by the City Engineer.
21. Prior to the commencement of any grading activity on-site, the applicant/contractor shall install structural control measures to reduce erosion and retain sedimentation. Measures may include, but are not limited to, stabilization of control entrance, temporary gravel and construction entrance, installation of a temporary gravel construction entrance, and installation of filter fabric fences. The applicant shall remove all temporary erosion control facilities upon stabilization of the entire project site, as approved by the City Engineer.
22. During construction, the applicant and the contractor of record shall exercise BMPs, such as daily street sweeping and the placement of erosion control measures on-site, to minimize storm water pollution. The contractor shall designate a primary contact person who shall be available to the City of Ukiah in the event of noted storm damage or storm event. Said person shall be responsible for inspection of all erosion control facilities.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

9. LAND USE AND PLANNING

Would the project:

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

The City of Ukiah is a compact urban environment, and functions as the County seat for Mendocino County. Commercial, residential and industrial land uses are planned for specific areas of the City as set forth in the 1995 Ukiah General Plan. Allowed/permitted land uses are defined via zoning districts as stipulated in the Ukiah Municipal Code. In this case, the subject property has a land use designation of Commercial and is located in a C-1 (Community Commercial) zoning district. The planned use of Proposed Parcel 1 as a commercial site (Rite Aid store) is allowed upon approval of a site development permit. The minor subdivision proposed parcels meet the minimum square footage requirement pursuant to the zoning code.

The proposed commercial use is an allowed use type in a C-1 zone upon approval of a major site development permit. The proposal is substantially consistent with the City's General Plan, the C-1 zoning district standards, and the Downtown Commercial Development Guidelines. The proposed development is also consistent with the CLUP Zone C criteria. (a,b)

As mentioned in Section 4 (Biological Resources), no habitat conservation plans or natural community conservation plans apply to the project site. (c)

10. MINERAL RESOURCES

Would the project:

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The subject property contains soils that have been historically altered and covered with structures and asphalt. The proposed minor subdivision and development of Proposed Parcel 1 with a replacement commercial structure is not expected to significantly impact natural resources in the area.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

The project site is not in an area containing any known mineral resources. The City's General Plan has not designated the site as a mineral resource area. No mining or other mineral extraction activities occur on the site. The project would not have an impact on mineral resources. (a, b)

11. NOISE	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project result in:				
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The subject property is located in a densely developed area of the existing urban landscape. This area is subject to typical background noise sources expected in an urban environment, including automobile and truck traffic, human voices, street work crews, heavy equipment, etc. The site is also subject to aircraft noise as mentioned previously in this report. The site location in the vicinity of the airport's northern approach/departure zone will expose people at this site to a moderate level of aircraft noise. The maximum noise levels projected for this site based upon information contained in the Airport CLUP is less than 55 CNEL on a moderate air traffic day, and less than 60 CNEL on a busy day. These noise levels, especially at short intervals as is common in this case, are not considered hazardous to humans. Aircraft noise may be a nuisance for future users of this site, however, noise impacts do not rise to the level of significance in this case.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

The City Noise Ordinance limits the maximum level of noise that can emanate from commercial units to 60 decibels during the hours of 10:00 p.m. to 7:00 a.m. and 65 decibels from 7:00 a.m. to 10:00 p.m. In this case, there is no evidence that the operation of the new Rite Aid store will cause these levels to be exceeded regularly or significantly. Noise impacts associated with the future development and use of Proposed 2 will be analyzed under a separate environmental review/

Short term project construction activities would likely lead to increased noise levels for the surrounding area. Noise associated with the project generally would be limited to construction equipment, typically consisting of water trucks, rubber-tired backhoes, trenchers, dump trucks, and other equipment. Potential sensitive receptors are the existing residential units located in the vicinity of the site adjacent to South Oak Street. Short term construction noise is generally treated differently from long term noise sources as it is a temporary source, especially for building construction, which typically lasts during the late spring and through the end of the summer months. Unless the construction noise occurs during period of normal sleep, it is generally considered to be less than significant. However, in order to avoid potential conflicts, a mitigation limiting the hours of operation/construction will address potential impacts associated with construction noise.

Noise associated with project operations would primarily occur with traffic visiting the site. Noise generated from the new Rite Aid store or the minor subdivision is not anticipated to be significant. This area should not experience a significant increase in noise levels associated with this proposal. (a, b, c, d, e, f)

Mitigation Measures:

- 23. The hours of construction shall be limited to 7:00 A.M to 6:00 P.M. Monday through Friday, and 8:00 A.M. to 4:00 P.M. on Saturday, with no construction to occur on Sundays and holidays.
- 24. Maximum level of noise that can emanate from long term commercial noise sources is limited to 60 decibels during the hours of 10:00 p.m. to 7:00 a.m. and 65 decibels from 7:00 a.m. to 10:00 p.m.

12. POPULATION AND HOUSING

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Discussion: The 2000 Census indicates that the population of Ukiah is 15,597 persons, with a slow and stable growth taking place over the past several years. The proposed subdivision and commercial development is not expected to adversely impact housing stock as the proposal is a replacement of a similar use. Subsequent development of Proposed Parcel 2 may add to the City's housing stock depending upon the type of proposal put forward in the future. (a, b, c)

13. PUBLIC SERVICES

Would the project result in:

Substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Fire protection?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion: The City of Ukiah is a relatively small urbanized area with a full complement of services including police and fire protection, emergency services, public schools, and public works and utilities.

The project will result in nominal impacts to municipal services. The proposed minor subdivision and the development of Proposed Parcel 1, the replacement of the old Rite Aid store with a new store, is not anticipated to impact area schools, nor will it place a demand on the City's parks or other public facilities.

The proposed project will not result in the need for new or expanded electrical generation sources. The City's sewer treatment plant is reaching capacity, however, City staff estimates are that hook-ups will be available at the time of construction of the new Rite Aid store. Development of Proposed Parcel 2 will require a separate analysis of capacity at the time of development.

The City of Ukiah Fire Department reviewed the project proposal and provided the following comments:

- ❖ Street numbers, door and exit signs, fire extinguishers, fire lanes and no parking areas, electrical shut-off access, secure lock box, fire alarms, smoke detectors and other standard requirements shall be annotated on required construction plans.
- ❖ Fire suppression sprinklers and a fire alarm system will be required in the Proposed Rite Aid structure to the satisfaction of the Fire Department.
- ❖ The subject project warrants the existence of three fire hydrants. Credit will be given for the existing fire hydrant at the Oak/Gobbi street intersection. Upgrading the existing fire hydrant at the Gobbi/South State Street intersection south of the project site shall be considered sufficient as meeting one fire hydrant requirement. The third (new) hydrant shall be located near the fire

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department connection (FDC) for the new Rite Aid building. The FDC shall be located near the street frontage as shown on the updated site plan.

Implementation of the above mitigation measures as recommended by the City of Ukiah Fire Department is necessary for efficient firefighting, thereby reducing the need for additional facilities, personnel or equipment. The project would have little impact on fire protection services. (a, b, c, d, e).

Mitigation Measures:

- 25. The City of Ukiah Fire Department shall review and approve a full set of construction plans prior to the issuance of building permits by the Building Division of the City of Ukiah Planning and Community Development Department. These plans shall include a sprinkler plan and an alarm plan. The site plan shall be updated to illustrate the location of the required fire hydrants. Plan checking and inspection fees shall be the responsibility of the applicant and shall be collected prior to issuance of permits.
- 26. The automatic extinguishing system (sprinklers) and the fire alarm system shall meet the standards of the Uniform Fire and Building Codes as approved by the City of Ukiah Fire Department.
- 27. Street numbers, door and exit signs, fire extinguishers, fire lanes and no parking areas, electrical shut-off access, secure lock box, fire alarms, smoke detectors and other standard requirements shall be annotated on required construction plans.

14. RECREATION

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The project does not propose the type of development which typically generates a demand for parks and recreational facilities. Should additional housing be proposed to be developed on Proposed Parcel 2 at some time in the future, impacts to parks and recreation will be analyzed. The project is not expected to have an impact on existing parks and recreational facilities within the City of Ukiah, nor would it require the construction or expansion of recreational facilities that may have a physical effect on the environment. (a, b)

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15. TRANSPORTATION/TRAFFIC

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that result in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: Ukiah is a rural city that is not experiencing significant population growth. However, as the government and commercial center for Mendocino County, the City has been growing in terms of commercial development, with a resultant increase in traffic and corresponding delays at intersections during the A.M. and P.M. peak hours. The adverse nature of these traffic impacts is somewhat subjective in nature, but recent work done as part of a city-wide traffic study shows that delays at some key intersections have substantially increased in recent years and that many of these intersections will require traffic signals or other traffic mitigations to retain acceptable levels of service. According to the Ukiah General Plan Circulation Element, the minimum acceptable level of service (LOS) on City commercial streets/intersections is LOS "D."

The proposed retail store development would be located at the intersection of South State Street and Gobbi Street. Both of these are arterial streets, with steady, but acceptable volumes of traffic past the site. According to the of the Citywide Circulation Study--Draft Final Report, prepared for the City in late 2006 by Omni-Means traffic consultants, this intersection operates at LOS C during both the AM peak hour and the PM peak hour.

Vehicle Traffic Impacts – Proposed Land Uses: Based on standard traffic generation calculations from the Traffic Generation Manual prepared by the Institute of Traffic Engineers, staff calculated that the average weekday daily traffic volume (ADT) for a pharmacy/drugstore with the 31,500 square feet of floor area of the existing Rite-Aid store is approximately 2,800 vehicle trips per day, with 101 trips occurring during the peak AM hour and 243 occurring during the peak PM hour.

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The pharmacy/drugstore proposed in this project is much smaller at 17,422 square feet and has a drive-thru pharmacy that typically generates less traffic than a store without this feature. In fact, using the ITE calculations, the projected ADT for the new store is approximately 1,536 vehicle trips per day, a reduction of approximately 1,300 trips per day. More importantly, the peak hour trips would decline to approximately 47 trips during the peak AM hour and 181 trips during the peak PM hour. Therefore, no substantial declines in traffic levels of service are anticipated for the abutting road segments or the State Street/Gobbi Street intersection are projected as a result of the drugstore/pharmacy development.

Vehicle Traffic Impacts – Projected Land Uses: No specific land use for the area vacated by the existing store has been included as part of this project, but staff finds it likely that it would be developed with retail stores, professional office, or high-density residential land uses. In the event that the site develops with office or retail land uses, staff estimates that the floor area would be approximately 15,200 square feet based on an allowed 40 percent buildout. In this case the projected ADT rates for office and general retail uses would be approximately 168 trips per day (24 peak AM/23 peak PM) and 618 trips per day (40 peak AM/75 peak PM), respectively.

Should the site be developed with residential land uses, up to 25 units could be constructed. The traffic generated from this number of housing units would be approximately 168 vehicle trips per day, with 13 trips during the peak AM hour and 16 trips during the peak PM hour.

The projected ADT rates are based on the most likely types of land uses, and staff notes that they could be substantially higher or lower than the traffic calculations above depending on the land use(s) that are actually developed on the site. However, for the purposes of analyzing potential traffic impacts, it is the opinion of staff that they represent a fair and reasonably accurate projection.

Vehicle Traffic Impacts – Proposed and Projected Land Uses: When the proposed and projected traffic generation calculations are combined, potential traffic from the site varies and staff has compared the projected traffic volumes in Table 1 below.

Table 1. Proposed and Projected Weekday Vehicle Trip Generation from Rite-Aid Site

Vehicle Generator	Average Daily Traffic (ADT)	A. M. Peak	P.M. Peak
Existing store	2800	101	243
Proposed store	1536	47	181
Proposed store plus:			
Apartments (25 units)	1704	60	197
Office	1704	71	204
Retail	2154	87	256

As this table shows, the combined traffic increases from the proposed drugstore/pharmacy and the projected land uses are generally lower than those that could occur from the existing store, with the exception of the PM peak generated by the new drugstore/pharmacy and new retail on the site. However, this difference is negligible and is not expected to cause any substantial decline in the LOS for the abutting roads or intersections.

Staff also notes that the proposed site design would allow for the abutting segment of Gobbi Street to be widened. This measure was required by the Department of Public Works to provide a more effective and safer alignment between this westbound segment of Gobbi Street and the westbound segment of Gobbi

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Street that runs adjacent to the Safeway store east of State Street. It will also allow the construction of a right-turn lane for the eastbound lane of Gobbi Street south of the project site. These improvements are expected to increase both the short-term and long-term LOS for the Gobbi Street/State Street intersection.

Parking: Based on the current parking standards of the Ukiah Municipal Code, 58 parking stalls would be required to comply with the Ukiah Municipal Code requirements for drugstores. In this case, 61 stalls are proposed and no additional parking stalls would be required.

Emergency Vehicle Access: The project site is located near major road segments and has multiple means of ingress and egress for emergency vehicles.

Mitigation Measures: None required.

16. UTILITIES AND SERVICE SYSTEMS	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The project will result in nominal impacts to municipal water and sewer services.

The City's Public Utilities Department has reviewed the proposal and commented as follows:

- ❖ Each lot is to be served by separate and individual water and sewer laterals. If the existing sewer lateral or laterals are to be used to serve either or both of the proposed parcels, the lateral shall be video taped and tested before the lateral may be reused.
- ❖ Capital Improvement fees for water service are based on the water meter size. Site development plans shall show the size and location of the proposed water services, including domestic, fire and irrigation services. Water meters shall be located in the sidewalk area either on South State Street or Gobbi Street frontages. All water and sewer construction shall conform to City of Ukiah Standard Plans. Capital Improvement fees and an estimate of cost of construction for water and sewer services to the property line are due at the time a building permit is issued.
- ❖ Sewer Connection fees will be required for this project. Credit will be given for the existing drain fixture units in the existing Rite Aid building that will be applied to the sewer connection fees for the new Rite Aid building. A request for and inventory of the existing drain fixture units must be made in writing to the Public Utilities Department prior to demolition of the existing Rite Aid building for said credit to be given. Any sewer connection fees associated with development on Proposed Parcel 2 will be determined at the time a building permit is issued for that development.

Mitigation Measures:

28. Each lot is to be served by separate and individual water and sewer laterals. If the existing sewer lateral or laterals are to be used to serve either or both of the proposed parcels, the lateral shall be video taped and tested before the lateral may be reused.
29. Site development plans shall show the size and location of the proposed water services, including domestic, fire and irrigation services. Water meters shall be located in the sidewalk area either on South State Street or Gobbi Street frontages. All water and sewer construction shall conform to City of Ukiah Standard Plans. Capital Improvement fees and an estimate of cost of construction for water and sewer services to the property line are due at the time a building permit is issued.
30. Sewer Connection fees will be required for this project. Credit will be given for the existing drain fixture units in the existing Rite Aid building that will be applied to the sewer connection fees for the new Rite Aid building. A request for and inventory of the existing drain fixture units must be made in writing to the Public Utilities Department prior to demolition of the existing Rite Aid building for said credit to be given. Any sewer connection fees associated with development on Proposed Parcel 2 will be determined at the time a building permit is issued for that development.

Staff notes, however, that these requirements are standard utility policies and standards and are not required to mitigate any significant, adverse environmental impact caused by the project. Therefore, they are not included in this analysis as formal mitigation measures.

MITIGATION MONITORING AND REPORTING: AB 3180 requires all public agencies to adopt a monitoring and reporting program whenever they adopt an EIR or "Mitigated Negative Declaration." The Mitigation Monitoring and Reporting Program for this Mitigated Negative Declaration, which is included below as Attachment 2 of this analysis, require the applicants to incorporate or comply with the important Mitigation Measures listed in the table (see Attachment 1).

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MANDATORY FINDINGS OF SIGNIFICANCE

NOTE: If there are significant environmental impacts which cannot be mitigated and no feasible project alternatives are available, then complete the mandatory findings of significance and attach to this initial study as an appendix. This is the first step for starting the environmental impact report (EIR) process.

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Does the project:				
a) Have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: Based on the review of the proposed project, site and surrounding areas, appropriate mitigation measures were identified in sections on Aesthetics; Air Quality; Cultural Resources; Geological/Soils; Hazards/Hazardous Materials; Hydrology/Water Quality; and Public Utilities.

Assuming implementation of the identified mitigation measures and standard conditions of project approval of the City of Ukiah and other pertinent agencies, no adverse impacts are anticipated and no additional mitigation measures are required.

RESOURCES USED TO PREPARE THIS ANALYSIS

1. City of Ukiah General Plan – General Plan Land Use Map, 1995
2. City of Ukiah Ukiah Municipal Code – Zoning Regulations
3. City of Ukiah Zoning Map
4. City of Ukiah Design Guidelines for Commercial Development, 2003
5. Applicant's written statement and application submittal.
6. Flood Insurance Rate Map (FEMA)
7. California Division of Mines and Geology. Fault Activity Map of California and Adjacent Areas (map). Issued 1994.
8. California Department of Toxic Substances Control. Hazardous Waste and Substances Sites List. www.dtsc.ca.gov/database/Calsites/Cortese_List.cfm.
9. California Integrated Waste Management Board. Solid Waste Information System (SWIS) database. www.ciwmb.ca.gov/SWIS/.
10. The Linkage Between Land Use, Transportation and Air Quality, State Air Resources Board, 1993.
11. The Land Use - Air Quality Linkage: How Land Use and Transportation Affect Air Quality, State Air Resources Board, 1997.
12. Transportation-Related Land Use Strategies to Minimize Mobile Source Emissions: An Indirect Source Research Project, State Air Resources Board, 1995.
13. A Source of Air Quality Conditions Including Emissions Inventory, Ozone Formation, PM10 Generation, and Mitigation Measures for Mendocino County, CA., Sonoma Technologies, Inc., November, 1998.
14. General Plan Revision and Growth Management Plan Technical Report: Natural Habitat Section, Michael W. Skenfield, and October, 1991
15. Soil Survey of Mendocino County, Eastern Part, and Trinity County, Southwestern Part, California, U.S. Department of Agriculture - Soil Conservation Service, January, 1991.
16. A History of the Salmonid Decline in the Russian River, Steiner Environmental Consulting, August, 1996
17. U.S.G.S. Topographical Map, Ukiah Quadrangle, 1958 (photo inspected 1975). Ukiah Municipal Airport Master Plan Report, Shutt Moen Associates, July, 1991
18. Citywide Circulation Study—Draft Final Report, Omni-Means Engineers and Planners, Fall, 2006
19. Traffic Impact Study for the Ukiah Medical Office Building—Revised Final, TJKM Transportation Consultants, May 3, 2005
20. Ukiah Municipal Airport Master Plan, adopted July, 1996
21. Mendocino County Airports Comprehensive Land Use Plan, County of Mendocino Planning Department, 1993
22. California Airport Land Use Planning Handbook, California Department of Transportation, Division of Aeronautics, 2002
23. Referral Comments from the following City staff and Agency representatives:
 - a. Chuck Yates, Fire Marshal
 - b. Paul Richey, Airport Manager
 - c. Liz Kirkley, Electrical Engineering Technician
 - d. Tim Eriksen, Public Works Director/City Engineer
 - e. Ben Kageyama, Senior Civil Engineer
 - f. Rick Sands, Public Utilities Associate
 - g. Trent Taylor, Police Sergeant
 - h. Chris Brown, Mendocino County Air Quality Management District

ATTACHMENTS TO THIS ANALYSIS

1. Recommended Mitigation Measures
2. Mitigation Monitoring Plan
3. Project Narrative prepared by Applicants
4. Site Plan for Existing Rite-Aid Store (reduced; not to scale)
5. Site Plan for Proposed Rite-Aid Store/Subdivision (reduced; not to scale)
6. Building Elevations (reduced; not to scale)

**RITE-AID STORE DEVELOPMENT AND SUBDIVISION MAP
Initial Study for Major Site Development Permit No. 06-19 &
Minor Subdivision Map No. 06-18**

Recommended Mitigation Measures

1. A schedule providing detailed information regarding the demolition of the existing drugstore building, the disposal or recycling of demolition materials, and the methods and timing for covering all exposed areas left by the demolition shall be submitted to the Ukiah Department of Planning and Community Development for review and this shall be approved by its staff prior to the issuance of any demolition permits for the building or for any site preparation or construction on either of the proposed parcels.
2. The number and size of freestanding and building signs shall be restricted to those prescribed by the Sign Code provisions of the Ukiah Municipal Code or permitted in a final sign program approved by the Ukiah Planning Commission as part of the discretionary permit approval for the project.
3. Prior to the issuance of any Building Permit for the construction of the retail store building, a Final Lighting Plan shall be submitted to the Director of Planning and Community Development or his/her designee for review for compliance with Ukiah Municipal Code standards for on-site lighting and with "dark sky" guidelines for reducing nighttime lighting on the site. The Final Lighting Plan shall include details regarding exterior lighting for structures, garden areas, and walkways, with lighting sources that are full cut-off, hooded, and down-cast, or otherwise shielded to ensure that light does not adversely shine towards neighboring properties, or toward the night sky. Additionally, all lighting shall be the minimum wattage necessary to provide adequate security, yet shall not result in excessively bright night glow. Sufficient details regarding the proposed wattage of all site lights shall be included in the Final Lighting Plan so that the Planning Staff can determine how bright the proposed site lights will be. The Director of Planning and Community Development shall have the authority to require the Final Lighting Plan to be modified (including the wattage) and/or additional information to be submitted so that the lighting meets the requirements listed above.
4. Construction activities shall be conducted with adequate dust suppression methods, including watering during grading and construction activities to limit the generation of fugitive dust or other methods approved by the Mendocino County Air Quality Management District. Prior to initiating soil removing activities for construction purposes, the applicant shall pre-wet affected areas with at least 0.5 gallons of water per square yard of ground area to control dust.
5. The burning of construction debris is prohibited. Any disposal of vegetation removed as a result of lot clearing shall be lawfully disposed of, preferably by chipping and composting, or as authorized by the Mendocino County Air Quality Management District.
6. During construction activities, the applicant/owner/contractor shall remove daily accumulation of mud and dirt on paved access lanes that serve the project site.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

7. The applicant/owner shall secure a grading permit from the Planning and Community Development Department and shall adhere to all grading permit conditions, including Best Management Practices. All areas disturbed by grading shall be either paved with concrete or asphaltic concrete, landscaping or erosion control hydroseed mixture.
8. All refuse generated by the facility shall be stored in approved disposal/storage containers, and appropriately covered. Removal of waste shall be on a weekly basis at a minimum so as to avoid excess waste. All trash receptacles/containers shall remain covered at all times to prevent fugitive odors and rodent infestation.
9. Demolition of the existing Rite Aid structure will require asbestos clearance and notification to the Mendocino County Air Quality Management District. The City of Ukiah Building Division cannot issue a demolition permit until they have received clearance from the MCAQMD.
10. Any stationary on-site internal combustion engines over 50 horsepower (i.e. generators) may require a permit from the MCAQMD depending upon fuel source and level of operation.
11. The use of the pharmacy drive-thru feature shall be limited to pharmacy customers only (i.e. no general retail customers allowed). Prior to use of the drive-thru feature, the applicant must demonstrate to the MCAQMD that vehicle emissions associated with the drive-thru feature will not contribute significantly to air quality in this area or the valley in general.
12. All grading activities must comply with MCAQMD Regulation 1 Rule 430 regarding fugitive dust emissions.
13. In the event that archaeological remains or artifacts are uncovered during construction activities, work shall be stopped and a qualified archaeologist shall survey the site with all costs borne by the applicant/owner. The archaeologist shall submit a report to the City Planning and Community Development Department with recommendations on the disposition of the site. Disposition may include, but is not limited to, excavation and documentation, capping the site, or leaving the site in an open space area. The recommendations of the archaeologist shall be incorporated in the project as project mitigation.
14. The proposed development shall be designed and constructed in accordance with the most current applicable Building Codes, including the Uniform Building Code (UBC) and the California Building Code (CBC) as determined by the Building Division of the Planning and Community Development Department of the City of Ukiah.
15. Should the proposed use of the site require the storage of materials equal or greater than 55 gallons of a liquid, 500 pounds of a solid or 200 cubic feet of compressed gas, the applicant will be required to submit a Hazardous Materials Business Plan to the Planning and Community Development Department with said plan to be renewed and updated annually. If quantities are less than the amounts noted above, the applicant must submit a Hazardous Materials/Waste registration form to the Department.
16. Equipment fueling and service shall be conducted at a designated location other than the project site, including local gas stations or repair shops.
17. Any spills resulting from fueling or hydraulic line breaks will be contained and cleaned up immediately.
18. During construction activities, equipment shall be maintained and kept in good operating conditions to reduce the likelihood of line breaks and leakage.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

19. No refueling or servicing shall be done without absorbent materials (i.e. absorbent pads, mats, socks, pillows, and granules) or drip pans underneath to contain spilled materials.
20. Prior to the issuance of building permits, the preparation of an on-site Grading and Drainage Plan, an Erosion and Sediment Transport Control Plan, and drainage calculations showing that the proposed drainage facilities are adequate shall be prepared by a Registered Civil Engineer for approval by the City Engineer. No drainage is allowed to be routed over sidewalks. Storm drain inlets must be protected by a fossil fuel filtering device if required by the City Engineer.
21. Prior to the commencement of any grading activity on-site, the applicant/contractor shall install structural control measures to reduce erosion and retain sedimentation. Measures may include, but are not limited to, stabilization of control entrance, temporary gravel and construction entrance, installation of a temporary gravel construction entrance, and installation of filter fabric fences. The applicant shall remove all temporary erosion control facilities upon stabilization of the entire project site, as approved by the City Engineer.
22. During construction, the applicant and the contractor of record shall exercise BMPs, such as daily street sweeping and the placement of erosion control measures on-site, to minimize storm water pollution. The contractor shall designate a primary contact person who shall be available to the City of Ukiah in the event of noted storm damage or storm event. Said person shall be responsible for inspection of all erosion control facilities.
23. The hours of construction shall be limited to 7:00 A.M to 6:00 P.M. Monday through Friday, and 8:00 A.M. to 4:00 P.M. on Saturday, with no construction to occur on Sundays and holidays.
24. Maximum level of noise that can emanate from long term commercial noise sources is limited to 60 decibels during the hours of 10:00 p.m. to 7:00 a.m. and 65 decibels from 7:00 a.m. to 10:00 p.m.
25. The City of Ukiah Fire Department shall review and approve a full set of construction plans prior to the issuance of building permits by the Building Division of the City of Ukiah Planning and Community Development Department. These plans shall include a sprinkler plan and an alarm plan. The site plan shall be updated to illustrate the location of the required fire hydrants. Plan checking and inspection fees shall be the responsibility of the applicant and shall be collected prior to issuance of permits.
26. The automatic extinguishing system (sprinklers) and the fire alarm system shall meet the standards of the Uniform Fire and Building Codes as approved by the City of Ukiah Fire Department.
27. Street numbers, door and exit signs, fire extinguishers, fire lanes and no parking areas, electrical shut-off access, secure lock box, fire alarms, smoke detectors and other standard requirements shall be annotated on required construction plans.

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

Attachment 2

**RITE-AID STORE DEVELOPMENT AND SUBDIVISION MAP
Initial Study for Major Site Development Permit No. 06-19 &
Minor Subdivision Map No. 06-18**

Mitigation Monitoring Program

MITIGATION MEASURES	MONITORING RESPONSIBILITY	HOW AND WHEN	VERIFICATION	FUNDING RESPONSIBILITY
Aesthetics Measure 1	Planning Department Staff	During all phases of construction	Planning and Public Works Staff	Applicants
Air Quality Mitigation Measures 4-12	Applicants & Grading/Site Preparation Contractors	During all phases of construction	Planning and Public Works Staff	Applicants
Cultural Resources Mitigation Measure 13	Applicants & Grading/Site Preparation Contractors	During all site preparation & construction phases	Planning Department staff	Applicants in the event of a discovery
Geology and Soils Measure 14	Applicants & Grading/Site Preparation Contractors	Prior to the submittal of building permits and during all phases of construction	Building Inspector	Applicants
Hazards & Hazardous Materials Measures 15-19	Applicants and Grading/site preparation contractors with oversight by Building Inspector, Public Works Staff & MCAQMD Staff	Prior to the submittal of building permits and during all site preparation and construction phases	Public Works & MCAQMD Staffs	Applicants
Continued on next page				

INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION

MITIGATION MEASURES	MONITORING RESPONSIBILITY	HOW AND WHEN	VERIFICATION	FUNDING RESPONSIBILITY
Hydrology & Water Quality Measures 20-22	Applicants and Grading/site preparation contractors with oversight by Building Inspector, Public Works Staff & MCAQMD Staff	During all site preparation & construction phases	Public Works Staff	Applicant
Noise Measures 23-24	Applicants and Grading/site preparation contractors with oversight by Building Inspector, Public Works Staff	During all site preparation & construction phases	Public Works Staff	Applicant
Public Utilities Measures 25-27	Fire Marshal	Prior to the issuance of building permits	Fire Marshal	Applicants

**RECEIVED**

DEC 01 2006

CITY OF UKIAH
PLANNING DEPT

December 1, 2006

Rite Aid Site Redevelopment / 680 South State Street, Ukiah

Rite Aid Corporation proposes to redevelop the existing Rite Aid site located at 680 South State Street in Ukiah, California. The existing zoning is C-1, which appropriate for the current and proposed retail use, so no change to the zoning or General Plan designation is anticipated. A minor subdivision of the property and a site development permit for the front portion of the project site are being submitted at this time, and appropriate permits will be sought for redevelopment of the rear portion of the property at a later time.

Minor Subdivision:

Rite Aid proposes to create two (2) lots via the Minor Subdivision process, and construct a new retail store, with associated parking, on the new lot closest to the intersection of South State and West Gobbi Streets (Parcel One, Sheet A0.2). The other lot (Parcel Two on Sheet A0.2) encompasses the area of the existing Rite Aid building footprint plus the travel aisle and parking adjacent to the south of that building (abutting West Gobbi and Oak Streets). Parcel One as proposed will be 68,488 square feet in size, while Parcel Two will be 38,001 square feet in size.

As a part of the subdivision process, City staff has indicated that dedication of additional right-of-way along West Gobbi Street will be required, in order to accommodate a bike lane for the length of that block and to improve vehicular circulation at the intersection. Utilities and traffic-control mechanisms at the northwest corner of the intersection will be relocated as needed to widen the turn-radius for vehicles turning right onto West Gobbi from South State Street, using a new right-of-way acquired by the City of Ukiah. The existing curb cut on Gobbi Street immediately west of South State Street will be closed, and a wide, landscape-lined pedestrian access to the new retail space will be created to access the new building, which will be situated much closer to the intersection than the existing building.

Site Development Permit:

The new retail building will be approximately 17,422 square feet, with 61 parking spaces, of which three (3) will be accessible. Said parking is located on three sides of the building, eliminating the one large parking area that currently exists at the front of the existing store, and spreading parking more evenly throughout the site. No parking will be allowed on the north side of the new building, which will accommodate a drive-thru pharmacy facility. Landscaping, which as proposed comprises approximately 15% of the site area of Parcel One, will consist of tree islands throughout the parking areas and landscaped areas along the perimeter of both the South State and West Gobbi Street frontages, and at the rear of



Parcel One along the common property line of Parcels One and Two. A detailed landscape plan has been developed for inclusion in the information provided to the Planning Commission.

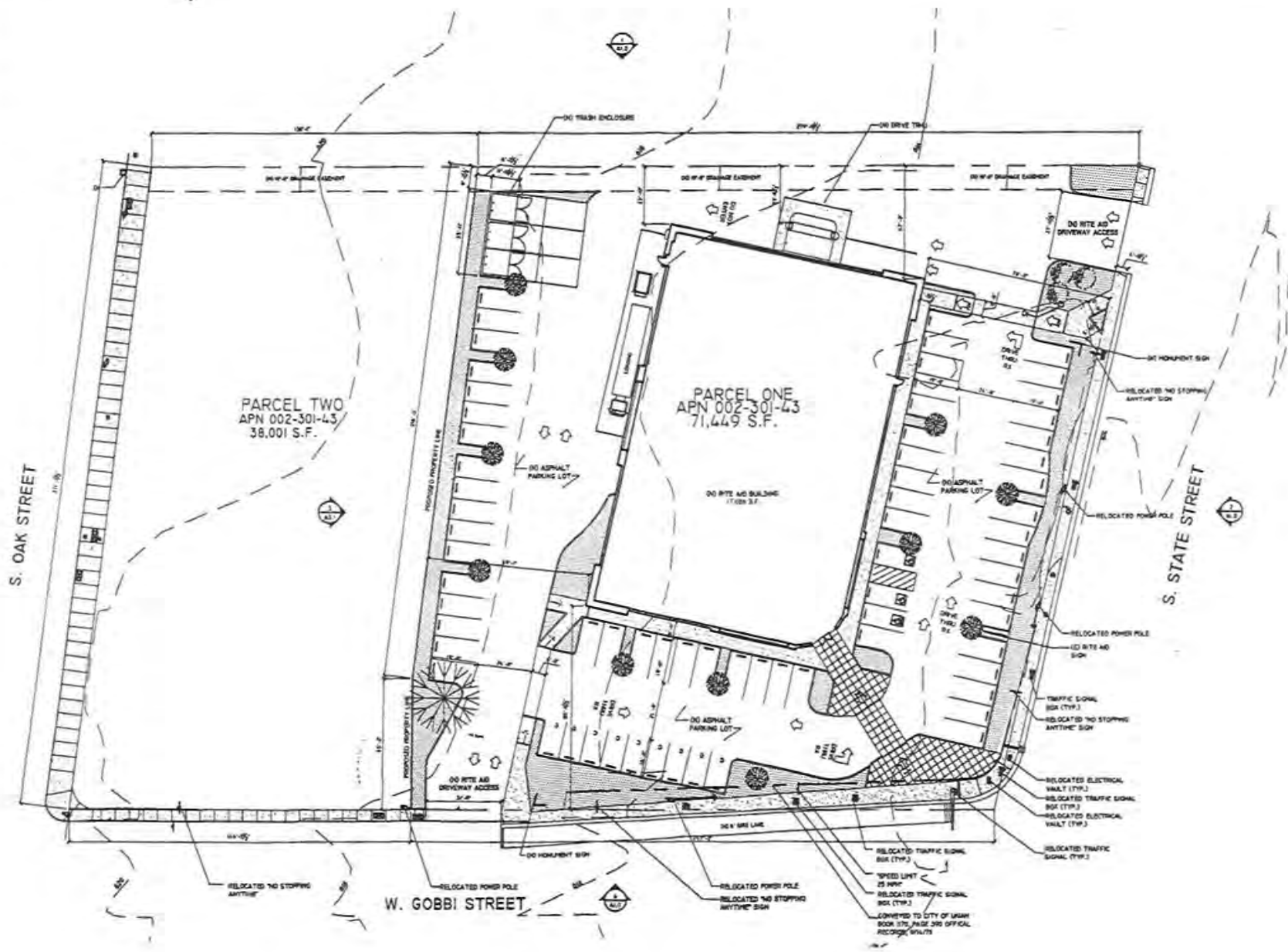
The building's interior layout is a standard corporate design, developed by Rite Aid and one of several floor plans used nation-wide. The new Rite Aid will feature a two-lane drive-thru pharmacy facility on the north end of the building, from which cars will exit by way of the West Gobbi Street curb cut located at the southwest corner of Parcel One.

The exterior design has been modified by Ruff + Associates, Inc. to incorporate some of the historic design elements thought to be characteristic of Ukiah. Building façade materials will be stucco and brick with stone trim. A cantilevered metal trellis is proposed on a portion of the east and south sides of the building to provide shade over the sidewalk. A color rendering and materials board has been provided to show proposed colors and materials selections.

Additional information on the monument sign, designed by Paramount Signs, has been included in this submittal package. We are still awaiting emissions information, to be provided by Rite Aid, and will forward that info to City staff immediately upon receipt.

7-399

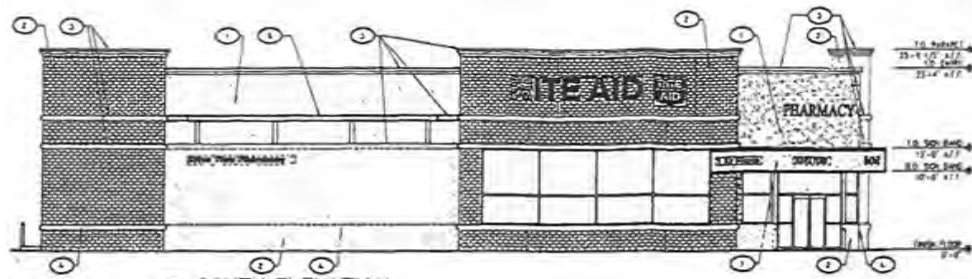
Attachment # 5



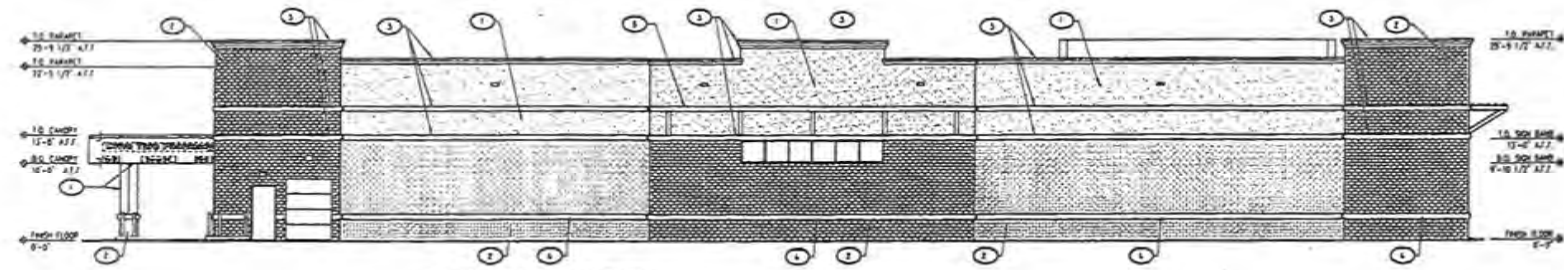
DATE	
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RUFF + ASSOCIATES Architecture Planning Development 100 West Sunday Street, Ukiah, CA 95482 Phone: 707-472-0115 Fax: 707-472-0377 e-mail: office@ruff.com COPYRIGHT © 2004 RUFF + ASSOCIATES, ALL RIGHTS RESERVED	
RITE AID CORPORATION NEW BUILDING 680 SOUTH STATE ST., UKIAH, CA 95482	
SHEET NO.	
SITE PLAN	
DRAWN BY	502
CHECKED BY	500

KEY NOTES (1)

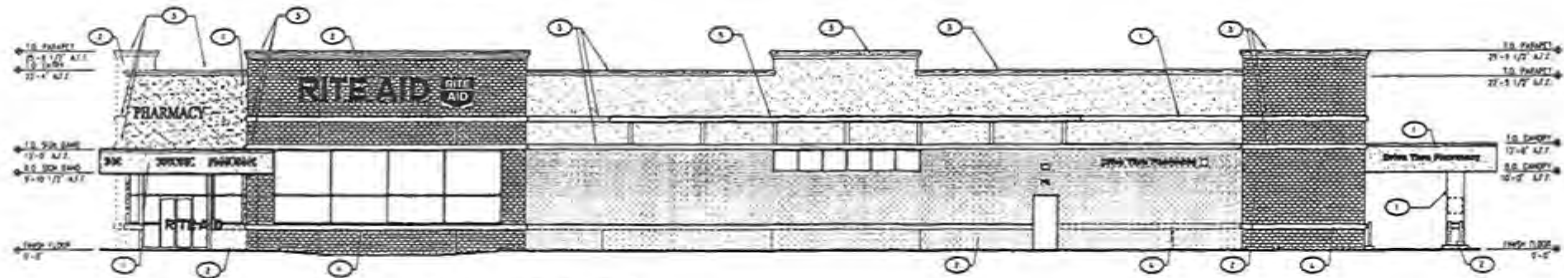
- (1) STUCCO FINISH
- (2) BRICK ROOFER
- (3) STAGES COVERED FORM TYP.
- (4) STONE HOLDING
- (5) METAL PANELS



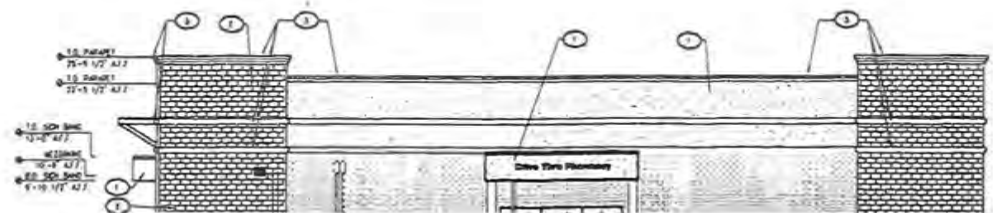
2 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



3 WEST ELEVATION
SCALE: 1/8" = 1'-0"



4 EAST ELEVATION
SCALE: 1/8" = 1'-0"



DATE	NO.	BY	CHKD.	APP.	REV.
 RUFF + ASSOCIATES Architecture Planning Development 100 West Sandley Street, Ukiah, CA 95482 Phone: 707-472-0335 Fax: 707-472-0337 e-mail: ruff@ruffandassociates.com					
					
RITE AID CORPORATION NEW BUILDING 680 SOUTH STATE ST., UKIAH, CA 95482					
SHEET NO. 1 SITE PLAN					